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Merton Council

Planning Applications Committee Agenda

Membership

Councillors:

Dave Ward (Chair)
Stephen Crowe (Vice-Chair)
Stephen Alambritis MBE
Billy Christie
David Dean
Nick Draper
Joan Henry
Simon McGrath
Carl Quilliam
Peter Southgate

Substitute Members:

Edward Foley
Edward Gretton
Najeeb Latif
Dennis Pearce
John Dehaney
Anthony Fairclough

Date: Thursday 28 April 2022

Time: 7.15 pm

Venue: Merton Civic Centre, London Road, Morden SM4 5DX

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<https://www.youtube.com/user/MertonCouncil>

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Planning Applications Committee Agenda

28 April 2022

- 1 Apologies for absence
- 2 Declarations of Pecuniary Interest
- 3 Minutes of the previous meeting 1 - 14
- 4 Town Planning Applications
The Chair will announce the order of Items at the beginning of the Meeting.
A Supplementary Agenda with any modifications will be published on the day of the meeting.
Note: there is no written report for this item
- 5 Carters Housing Estate, Raynes Park, SW20 15 - 54
Application No: 20/P2638
Ward: Raynes Park
Recommendation: Grant Permission subject to conditions
- 6 Electrical sub station adjacent to 14 Pepys Road, Raynes Park, SW20 8NH 55 - 88
Application No: 20/P2583
Ward: Raynes Park
Recommendation: Grant planning permission subject to conditions and s106 agreement.
- 7 Electrical sub station adjacent to 14 Pepys Road, Raynes Park, SW20 8NH 89 - 120
Application No: 21/P3428
Ward: Raynes Park
Recommendation: Grant planning permission subject to conditions and S106 agreement.
- 8 2A Trinity Road, Wimbledon, SW19 8RL 121 - 164
Application No: 21/P3215
Ward: Trinity
Recommendation: Grant Planning Permission subject to conditions and completion of a S.106 Agreement.
- 9 Planning Appeal Decisions 165 - 170
Officer Recommendation:

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That Members note the contents of the report.

10	Planning Enforcement - Summary of Current Cases	171 -
	Officer Recommendation:	176

That Members note the contents of the report.

Note on declarations of interest

Members are advised to declare any Disclosable Pecuniary Interest in any matter to be considered at the meeting. If a pecuniary interest is declared they should withdraw from the meeting room during the whole of the consideration of that matter and must not participate in any vote on that matter. For further advice please speak with the Managing Director, South London Legal Partnership.

Agenda Item 3

All minutes are draft until agreed at the next meeting of the committee/panel. To find out the date of the next meeting please check the calendar of events at your local library or online at www.merton.gov.uk/committee.

PLANNING APPLICATIONS COMMITTEE

17 MARCH 2022

(7.15 pm - 11.00 pm)

PRESENT Councillors Councillor Dave Ward (in the Chair),
Councillor Stephen Crowe, Councillor Stephen Alambritis,
Councillor Billy Christie, Councillor Nick Draper,
Councillor Joan Henry and Councillor Peter Southgate Councillor
Najeeb Latif and Councillor Ben Butler

ALSO PRESENT Tim Bryson (Development Control Team Leader North)
Stuart Adams (Development Control Team Leader South) Amy
Dumitrescu (Democracy Services Manager) Bola Roberts
(Democratic Services Officer)

**ATTENDING
REMOTELY** Lesley Barakchizadeh (Interim Building and Development
Control Manager)
Tim Lipscomb (Planning Officer)
Sarah Attanayake (Transport Planning Project Officer)

1 APOLOGIES FOR ABSENCE (Agenda Item 1)

Apologies for absence were received from Councillors Dean and Quilliam.
Councillors Latif and Butler attended as Substitutes. Apologies were also received
from Councillor McGrath and apologies for lateness were received from Councillor
Henry.

2 DECLARATIONS OF PECUNIARY INTEREST (Agenda Item 2)

There were no declarations of interest.

3 MINUTES OF THE PREVIOUS MEETING (Agenda Item 3)

RESOLVED: That the minutes of the meeting held on 10th of February are agreed as
an accurate record.

4 TOWN PLANNING APPLICATIONS (Agenda Item 4)

The Committee noted the amendments and modifications to the officer's report. The Chair advised that items would be taken in the following order items 7, 9, 5, 6,10 and 8. For the purposes of the minutes the items are minuted in the published agenda order.

5 26 HAREWOOD ROAD LONDON, SW19 2HD (Agenda Item 5)

The Planning Officer presented the report

The Committee received verbal representations from two objectors who made points including:

- That the first application had been rejected for failing to meet minimum standards for bedrooms; the present application only provided a corridor for access and there would not be an improvement in the living area
- There are no applications like the one proposed and separating the existing garden into 3 separate areas would set an undesirable precedent and is out of character with the surrounding area
- The flat ceiling in the living area fell below the minimum height of 2.3 meters and the national requirement set out for space
- The Objector felt that the proposed site would be used effectively for 2 flats, one 3 bedroom and a 2 bedroom. This would allow occupation of a family which would prevent the need to separate the garden
- The Objector had raised concerns to the Councils Planning Department that the border between his house and the site was incorrect and the application should not be approved
- The access to the bin storage would be significantly decreased
- the developers did not add in a third flat, the objector requested that a condition should be put in to prevent the sale of the property if converted and rented as a 2-bedroom flat in future
- The objector stated that residents were concerned that the development was based on financial gain, whilst limiting the living space
- The Objector raised concerns regarding the sewer, which would be burdened with the proposed development, stating that the company Dyno-rod had been called out 4 times in the last 12 months due to blockages

The Applicant made points in response including:

- The Applicant had worked with Planning Officers on this site to mitigate concerns raised in the original application
- The property would be reinstated to proper use to 3 units as this was previously empty, the unit would meet Merton's building target
- The Agent reported that height and mass elevation noted by Planning Officers was conservative and fitted in with permitted development
- The rear extension was minimal in relation to what existed already
- Space requirements were met in accordance with the Planning Policy

Councillor Attawar read a statement on behalf of the residents in Harewood Road against the planning application. Councillor Attawar stated:

- Residents had raised concerns on size, mass and height.
- If the dwelling was converted into 2 flats, then it would be adequate.
- Having family homes is more desirable in Merton.
- There were several inconsistencies especially in relation to bin storage.
- The Committee should consider that it fulfils the policy of creating good housing for Merton's residents and not targets.
- The Developer should reconsider the design

The Planning Officer responded to points raised:

- The Planning Officer confirmed that the site is making use of optimum space and met minimum space requirements
- Each flat would have access to gardens which is uncommon in this kind of development and would not be harmful to the area
- The Plans were as set out in the modifications sheet. If The Committee were to approve planning permission, then the developers could be asked to revisit the plans

In response to questions from members, the Planning Officer advised:

- Existing head height would be maintained
- The removal of chimneys would not need planning permission and would be covered under building control regulations
- The developer would need to seek the neighbour's permission to carry out the works relating to the removal of a chimney
- If the plans were not accurate it would affect the building style and if members approved the application, developers would be asked to revisit the plans and designs.

The Chair moved to the vote and it was

RESOLVED

That the application be granted subject to conditions and completion of a Section 106 legal agreement.

6 16-20 MORDEN ROAD, SOUTH WIMBLEDON, SW19 3BN (Agenda Item 6)

The Planning Officer presented the report.

The Committee received a verbal representation from one objector who made points including:

- A similar application had been refused two years ago and this had been brought back with an additional 25 flats and more single aspect dwellings

- There was no separation between residential and commercial waste and services
- The scheme offered no affordable housing
- Changes are limited to the ground floor and the previous grounds for rejection were still relevant to this application where similarities remained.
- The application did not comply with the London plan
- The application could not provide comfortable living for future residents with its current plans
- The Objector raised concerns on the application not providing light ventilation in all habitable rooms and kitchens
- The Objector found the application wanting in provision of contextual design narrative
- The Objector urged the Committee to reject the application especially as the sites sits on traffic thoroughfare

A statement was read on behalf of Councillor Benbow. Councillor Benbow stated that the application was in poor design and did not provide affordable housing. The architectural concept is of poor design and the height would cause overlooking and loss of light to the neighbouring residents. Pollution of noise traffic would be distressing to residents. There was no mention of air quality and the developer had failed to contact the local Police to discuss secure by design.

The Planning Officer responded and made comments including:

- That this application differed to the previous application significantly
- The Urban Design Officer had raised no significant concerns on the application and the comments were noted by the developer
- The application allowed for signage for commercial units
- Units on the ground floor are single aspect as residential units are at the rear and the single units are facing East or West
- The bin storage area is large for the size of the development and developers can sub-divide the space or manage the refuse collection from one spot
- The comments from the Planning Inspectorate related to the ground floor, which is set aside for commercial use and not residential
- Conditions can be placed in relation to mechanical ventilation
- The Planning Officer confirmed the need for 40% housing and 35 fast tracked which is subject to a financial viability assessment
- The point raised for loss of light was not a concern for refusal from the previous application and bulk and mass are the same as the previous application
- The request for secure for design has been noted and a condition placed for details like air locked doors

Members raised questions on affordable housing on the site. The development is close to the streets and amenities and flats can be easily sold, so would be able to achieve viability to accommodate affordable houses.

The Planning Officer responded to further questions from members:

- The Planning Officer advised the Committee that the costings are available to view on the Council's website. There was a contrast between the applicants and the council's viability report as they had different views in concluding the viability report. The Schemes viability report showed a deficiency of £3 million whilst the Council's viability report showed a deficiency of £500,000
- The report is still not viable. The policy is subject to a get out clause and the applicant has provided a viability report, which was subject to scrutiny by an independent assessor who ruled the scheme not viable
- The Planning Inspectorate decision was a planning consideration and cannot be overlooked, it was noted that the single aspect scheme was acceptable but noted the East and North facing windows. The current application has taken into consideration concerns raised and conditions placed. Planning Officers considered that the application has overcome the concerns.

The proposal to refuse was proposed and seconded and put to the vote. The vote fell and The Chair moved to the officer's recommendation to grant permission

The Chair moved to the vote, it was

RESOLVED

That planning permission was granted subject to conditions and section 106 agreement.

7 RUFUS BUSINESS CENTRE, RAVENSBURY TERRACE, WIMBLEDON PARK, LONDON, SW18 4RL (Agenda Item 7)

The Development Control Leader (North) presented the report.

The Committee received a verbal representation from one objector who made points including:

- The independent assessors/ financial viability report had only been made available to the public 36 hours prior to the meeting
- The report contained estimated costs which were high and missing vital information which the community objected to such as the height, bulk and no road link to Wellington works
- The application should not be approved without an independent assessor verifying the abnormal cost and sharing this information with the public
- The objector spoke on the illegal use of asbestos on Rufus as this was a separate site to that of Hazelmere. The contamination risk was low, yet inflated costs were given
- The developer had not confirmed what radiological remediation occurred in Hazelmere

- The objector suggested to the Committee that there be an undertaking of survey or measurement works carried out on the site for a minimal cost and that the remediation work cost should be made known to members of the public
- The objector suggested that to reach full potential and maximise development work in the area, the developers should develop Rufus and the adjacent site.
- Providing vehicle access through Rufus to Wellington Works would achieve maximised development by providing safe access
- The developers design access statement confirmed providing vehicular access was achievable but would cost a considerable amount, the objector stated this could be mitigated by savings from construction and remediation
- The objector had obtained over 350 signatures for a petition for the road link; this would be beneficial for Merton Council in terms of development and residents in terms of road safety and children of Merton Park Primary, by taking operational and construction vehicles out
- The developer produced an analysis road link, which the objector said was flawed and that the developer did not look at other options
- The objector said that the proposal should be rejected and redesigned for vehicle access safety the agenda notes states this
- The scale and mass of the development was not in keeping with the area and the Merton urban Design Officer had stated that form and masses do not relate well to adjacent development to the North
- The Urban Greening, fell below the GLA threshold
- The objector stated that height should be reduced, and green spaces increased in consideration for amenities.

The Agent to the Applicant responded and made points including:

- In relation to matters raised at the previous Committee meeting, clarification was sought for identified radiation contamination one was for the incandescent mantel and radio works addressed in 7.5 22 of the Officers reports identified contamination levels to Rufus site to the Northeast
- The Agent to the applicant was confident of doing remediation works to standard, as his team undertook remediation work on the adjacent site to Rufus so have acquired the knowledge
- In terms of cost the Agent to the applicant stated that the costings had been directed to officers and the costs analysis have now been independently assessed by professional advisors to circa £1.8 million pounds and the cost of works on the adjacent site Hazelmere costs is £3.99 million
- The high cost of the remediation is reflected in the cost of affordable housing this currently dictates the amount of affordable houses in the development. However, the numbers of affordable homes will be reviewed later as the development progresses. The Agent to the applicant hopes the Committee is reassured by the mechanism
- In addition, the affordable homes will have low rent and no shared ownership, the cost of the development is under £490,000
- Service charges will be kept to a minimum as zero carbon is reducing; details are highlighted in the report contained in the agenda pack

- The building will be close to blue badge parking, and this will include affordable parking to promote integration
- The building will be built to a high standard and there is no distinction between affordable and private homes hence eliminating segregation
- The Agent to the applicant confirmed that the development would have access for emergency services. This concern was raised by the Committee at the previous planning committee
- The development addresses the London Plan for 906 new homes. The site will provide energy sufficient short term tenanted homes
- Clean up cost of contaminated site will be revisited to establish how many more affordable homes could be included in the development

The Development Control Leader (North) responded to points raised and advised Members that

- The viability report had been assessed by an independent assessor, part of the process was to reduce cost and this was included in the report
- Affordable housing was attached to the same block as private residents
- The Officer advised the Committee that block D should remain as it is, and the housing provider will be in charge of the block, they delivered on the adjacent site
- Access to adjacent does not warrant vehicle access as this would result in loss of units and potential flood risks, what is proposed is future pedestrian and cycle access.

In response to Members questions The Development Control Leader (North) advised

In terms of services charges, this related to services within the building itself, such as lifts and hallways

In terms of contamination figures within the report the Planning Officer confirmed that this was present from the start, the new figures quoted was in relation to the adjacent (Hazelmere) site and not the current application as part of the late stage review that member received recently

The costings were subject to investigative work just within the application site

The Planning Officer confirmed that the design of block D was different and will have its own separate energy supply apart from the other block

The Planning Officer confirmed that investigative remediation work is standard and will always occur

The Planning Officer confirmed, that as part of the late-stage review, if there were a reduction in the cost, then there would be claw back of funds that would go towards affordable housing.

In response to further questions The Development Control Leader (North) explained that the benefit of dual ventilation as opposed to single aspect is cross ventilation and light.

Members made comments on the application. The development had potential to provide more affordable housing, which was in line with the London Plan. The viability appraisal was not known. It was proposed and seconded for the application to be brought back so that the developers could produce an application that was more forthcoming in viability and include more affordable flats. Members asked for more information on the contamination

The Development Control Leader (North) advised the Committee that

The true costs cannot be realised until development starts, as a result, the costings from a neighbouring site was submitted to help demonstrate costings for site remediation

There was nothing to bring back to the Committee if the application was deferred

Members made further comments on the application in relation to affordable housing and costings.

As a result of members comments the Chair stated that as it was the will of the Committee the vote would be to defer to a future meeting with reasons

The Chair moved to vote, and it was

RESOLVED:

That the Application be deferred to a future Committee Meeting, to allow members seek further information on financial viability, contamination and whether more affordable housing could be built.

8 225 STREATHAM ROAD, STREATHAM, LONDON SW16 6NZ (Agenda Item 8)

The Planning Officer presented the report

The Applicant spoke and made points in support of the application including:

- The applicant had cleaned up the site and removed asbestos and ensured travellers vacated the site
- The Applicant informed The Committee that the Council closed down the site without prior notice
- The Applicant put Planning permission for a car wash
- The Applicant stated that the Objectors opposing the Applicants site did not live in the vicinity and this was against the Law

A statement was read out on behalf of the Ward Councillor Linda Kirby who made points including:

- The site had two approved housing developments from 2017 and 2019
- The Applicant had left the site in a slum like state
- The Applicant had built a car wash business
- The Enforcement Officers had closed down the business as the site was not used for the purposes of the planning permission which had been granted
- The Applicant had put in a retrospective application to build houses, but this did not materialise
- The business had put in a retrospective order for planning permission and continued building during weekends which had disturbed the neighbours
- The area has two car wash businesses and another one is not needed
- The behaviour of the business has shown no regard for the residents

The Planning Officer for responded to points raised including:

- A temporary stop notice had been issued by the Enforcement Team and this was considered as a written notice
- The notice was for unauthorised use and the Planning Officer welcomed the Applicant's offer to meet with him

The Planning Officer for responded to questions from members:

- The site had double yellow lines around vehicle access and parking bays
- The proposal would cause traffic congestion for cars

Members commented on the application. Members commented were mindful of Officers recommendations but if the Application could attend a meeting to mediate on a matter of housing development, then the application could be deferred.

The Planning Officer responded to Member's comments and advised:

- The application had been submitted in January 2022
- The application transport statement did not relate to the application but a previous one submitted. There was no noise impact statement, so no clarity was given by the Applicant
- The applicant has indicated a request to meet but not confirmed to discuss this has not been put in writing. A temporary condition could be given
- The applicant needs to resolve the breach on the land for an invitation to put in a new application.
- If members believe there is scope for the application, then the application could be deferred.

The Chair moved to the vote and it was

RESOLVED

That the application was deferred to a future Planning Committee Meeting.

9 BENNETS COURTYARD, WATERMILL WAY, SW19 2RW (Agenda Item 9)

The Planning Officer presented the report

The Committee received a verbal representation from two objectors who made points including

- The Objector commented that the Committee take into consideration the importance of the comments of the conservation Officer for Merton. The Officer had advised that an additional floor proposed would not be an enhancement to the existing building, the objector said the comments were critical. The objector read from the officers' comments citing the councils' policies relating to the development. Policies NPPF, CS4 and CS14, DND 2 and 4 which advises on weight, enhancement, conservation and design in the Wandle Valley area
- The objector mentioned that the scheme proposed 15 flats none of which were affordable dwellings
- The Objector did not agree with the Planning Officers report to conserve and believed that the policy requirement is for enhancement
- The Objector pointed out that the Conservatives Officers report was not at hand for the Committee to review at the last Planning application meeting. The report contained Conservation and heritage views
- The Objector urged the voting Committee to look at the significance of the Conservation report
- The Objector mentioned that heritage views took precedence over other considerations
- The Objector felt that the planning laws did not protect heritage and conservation
- The Objector said that the Conservatives Officers found the development did not enhance but detracts
- The Objector advised the Committee that heritage laws required that the proposal be rejected and voted against approval of the application

The Agent for the applicant made comments including

- The report had gone through the scrutiny of lawyers on behalf of the applicant
- The Conservations report addressed the conservation area and the effect of the building on which the extension will go
- Paragraph 1.5 of the report quotes the statutory tests of the conservatory areas, which in section 72 requires that attention be paid to the desirability of preserving or enhancement, this fulfils statutory test requirements
- Planning Officers had concluded that there was a neutral effect in relation to the heritage asset and this met the statutory test
- Planning Officers had set out the right tests, the full views of the Conservation Officer were set out, Planning Officers concluded that no harm would be caused to the asset and had made recommendations
- The Agent to the Applicant informed the Committee that the report addressed reasons given for why it was quashed by agreement on ground 1, failure to include Conservation Officers recommendations

- The Agent to the Applicant reported that the site is in a growth area, which aims to develop new homes in future, making good use of land, which the proposal fulfils
- The Agent to the Applicant urged the Committee to revisit the last resolution made and reconsider. The Councils growth expectations can be delivered

In response to members questions raised the Planning Officer advised that

- The applicant would have to submit a viability report, in terms of affordable housing, this was assessed by the Councils independent assessor
- There are four single aspect units and the rest are dual aspect.

The Planning Officer clarified a point on the Conservation Officers report and to set out reasons why those steps were taken. That the Conversations Officers comments should be amalgamated with Planning Officers report, as opposed to being reported independently. This was the reason why the Conversations Officer report was not included. Going forward the report will now be included. The Planning Officer further went on to say that the single aspect unit follows on from the layout of the floors below.

Members continued with comments, noting that putting up another unit detracts and affects the view of surrounding buildings in the conservation area and the design did not enhance but detracted

The chair moved to the vote and it was

RESOLVED:

1. That the Planning Applications Committee REFUSED the application for the following reasons:
 - That the application detracted from the conservation area
2. DELEGATED to the Director of Environment & Regeneration the authority to make any appropriate amendments in the context of the above to the wording of the grounds of refusal including references to appropriate policies

10 41 – 47 WIMBLEDON HILL ROAD, WIMBLEDON, LONDON, SW19 7NA
(Agenda Item 10)

The Development Control Team Leader (North) presented the report

The Committee received a verbal representation from one objector who made points including:

- In regards to the location of the entrance, there was no CCTV and this would pose a safety issue for returning female guests
- Due to lack of security the Police had recommended an onsite concierge

- The Objector raised concerns on the access which would disturb local residents if guests arrive and depart 24 hours a day
- The Objector would like the development to have onsite security as this would prevent any illegal activities being carried out

The Applicant spoke in response and made points including

- That the current application follows a previous application smaller than the previous scheme in 2019, which has been changed due to the current climate for hotels
- The scheme has existing tenants, the developers are looking to work with other schemes such as Go native to produce a viable scheme
- Local residents welcomed the scheme as somewhere family members and friends could stay when visiting Wimbledon
- The access has been maintained and the old elements of the scheme stripped back
- The developers are looking to reinstate the building to be more viable and serving the local community

The Development Control Team Leader (North) addressed concerns by the Objectors points including:

- The Police commented on the scheme and recommended conditions which has been placed in the Planning report recommendations

The Planning Officer responded to Members questions

- The Planning Officer could not place conditions on the internal security of the building as it was an operational aspect
- Modern shop fronts windows would be put in place and conditions for safeguarding shopfront designs
- The Planning Officer did not have figures on employment the scheme would generate.

Members made comments on the application. Members commented on the front visual design proposed for the development which was a good improvement to the existing one Members appreciated the apartment style hotel and not guided by lobby front but to just access the apartments by themselves'

The Chair moved to the vote and it was

RESOLVED

That Planning permission was granted subject to conditions and Section 106 agreement.

11 PLANNING APPEAL DECISIONS (Agenda Item 11)

The Committee noted the report

12 PLANNING ENFORCEMENT - SUMMARY OF CURRENT CASES (Agenda Item 12)

The Committee noted the report

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PLANNING APPLICATIONS COMMITTEE 28 April 2022

Item No:

<u>UPRN</u>	<u>APPLICATION NO.</u>	<u>DATE VALID</u>
	20/P2638	20/08/2020

Address/Site Carters Housing Estate, Raynes Park, SW20

(Ward) Raynes Park

Proposal: Installation of 45 free standing bin storage units to hard landscaped areas involving the loss of 16 formal on-street car parking spaces and 14 informal car parking spaces across carter's estate (total of 388 parking spaces reduced to 358).

Drawing Nos: CHG-CE-P-LP-P, CHG-CE-P-BPO-P Rev B, PBLX-2, PBLX-3, PBLX-4, PBLX-6, CHG-CE-P-BP1-P Rev B, CHG-CE-P-BP2-P Rev B, CHG-CE-P-BP3-P Rev B, CHG-CE-P-BP4-P Rev B, CHG-CE-P-BP5-P Rev B

Contact Officer: Tim Lipscomb (0208 545 3496)

RECOMMENDATION

Grant Permission subject to conditions

CHECKLIST INFORMATION

- Heads of Agreement: No.
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: Yes (major application)
- Site notice: Yes (major application)
- Design Review Panel consulted: No
- Number of neighbours consulted: 876
- External consultations: Yes
- Flood Zone - No
- Conservation area: No
- Listed buildings: No
- Tree protection orders: No
- Controlled Parking Zone: No
- PTAL: 1a-2 (poor) (entrance to the estate is PTAL 3-4)

1. **INTRODUCTION**

- 1.1 This application is being brought to the Planning Applications Committee for determination due to the nature and scale of the development and the number of objections.

2. **SITE AND SURROUNDINGS**

- 2.1 The site comprises the Carters Estate, bounded to the west by the A3, to the north by a railway line and to the east by the B282. To the south of the site is warehouse style retail units along Bushey Road (occupied by 'Next' and 'Pets at Home') with surface level car parking and West Wimbledon Primary School.
- 2.2 The site includes Bodnant Gardens, Savill Gardens, Stourhead Gardens, Polesden Gardens, Petworth Gardens, Hidcote Gardens, Nymans Gardens, Farnham Gardens and Grayswood Gardens. The estate is managed by Clarion Housing Association.
- 2.3 The estate comprises rows of flat roof, two-storey dwellings and three storey flatted blocks, laid out in terraced blocks, with a distinctive architectural form typical of the 1960s, exhibiting a utilitarian form with brickwork at ground floor level and a range of colours of vertical slate or tile hanging/cladding at first floor level. The site also accommodates the 4 storey Carter House Nursing Home building, to the junction of Bodnant Gardens and Farnham Gardens. Landscaped courtyards are dotted around the site with two larger, linear greens spaces running north to south within the estate.
- 2.4 A number of the residential dwellings have individual driveways to the frontage of the properties and/or integral garages (although it is unlikely that many garages are used for parking of cars as the garages are relatively narrow). A number of integral garages have been converted into habitable accommodation. On street parking is available in parking bays and on other areas of the roads, which are not restricted by double yellow lines (double yellow lines are in the vicinity of junctions only).
- 2.5 There are a total of 422 residential units within the site with 388 parking spaces on site currently (a parking ratio of 0.92 spaces per unit).
- 2.6 Refuse and recycling storage is in wheelie bins on the individual driveways of houses and within wheelie bins for communal use are to the frontage of the flatted blocks, within the approach route to the entrances to the building, in addition to the internal bin storage areas in the flats. The whole estate is on a fortnightly wheelie bin collection which alternates between refuse/paper & card recycling one week and dry mixed recycling the next. Food waste is collected weekly.
- 2.7 The site is subject not subject to any specific planning constraints and is not within a Controlled Parking Zone (however, signage indicates that parking in bays is for residents only). The site has a PTAL range of 0-4. The access into

the estate at Bodnant Gardens has a PTAL of 4, this drops to 1b for the majority of the estate and 0 for the westernmost parts of the estate.

3. CURRENT PROPOSAL

3.1 The application seeks permission for the provision of 240 freestanding bin stores around the site, in 45 groups (or banks), to be located in areas which previously accommodated car parking, or on hardstanding areas adjacent to raised planters and fences/walls. These freestanding communal bin stores would replace the existing communal bins that the householders living in the flatted units currently have.

3.2 As set out above, there are a total of 422 residential units within the site with 388 parking spaces on the site currently. The overall level of car parking on the site would be reduced to 358 spaces, a reduction of 30 spaces. In terms of overall parking provision, the existing ratio is 0.92 spaces per dwelling, with the proposed ratio being 0.85 spaces per dwelling.

3.3 The proposals for each street in the estate are set out below:

Bodnant Gardens

2 informal parking spaces lost.

1 bank of bins at no.38 (3 bin units)

1 bank of bins at no.24 (3 bin units)

Farnham Gardens

5 parking spaces on off-street parking bays lost.

2 informal parking spaces lost.

Provision of 11 banks of bins (33 bin units)

Provision of 2 banks of bins (4 units) between nos. 31-35 and nos.12-14)

Provision of 1 single bin adjacent to Carter House.

Grayswood Gardens

1 informal parking space lost

1 single bin unit proposed

Hidcote Gardens

No changes proposed

Nyman Gardens

No changes proposed

Petworth Gardens

Provision of 2 banks of bins (4 units)

Polesden Gardens

7 parking spaces on off-street parking bays lost.

5 informal parking spaces lost.

Provision of 13 banks of bins (39 bin units)

Provision of 2 banks of bins (4 units) between nos. 93-97 and nos.12-14)

Provision of 2 banks of bins (4 units) between nos. 8-12 and nos.13-15)

Savill Gardens

1 informal parking space to be lost.

1 single bin unit proposed

Stourhead Gardens

4 parking spaces on off-street parking bays lost.

3 informal parking spaces to be lost.

Provision of 5 banks of bins (16 individual bins) adjacent to the road. 1 bank of bins adjacent to nos.43-47 (3 bin units).

- 3.4 The bin storage would be to serve flatted units on the estate only, with the houses continuing to have individual refuse and recycling collections.
- 3.5 The applicant has confirmed that the existing internal bin storage cupboards would be closed off once the new facilities are installed.
- 3.5 Four bin storage types are proposed:
- PBLX-2 1490mm height, 1374mm width, 1062mm depth
 - PBLX-3 1490mm height, 2037mm width, 1062mm depth.
 - PBLX-4 1490mm height, 2656mm width, 1062mm depth
 - PBLX-6 1490mm height, 3938mm, 1062mm depth
- 3.6 The bin storage units have curved roof profiles. They have robust galvanised steel frames (silver), with steel panel cladding (black finish) with a fireboard MgO core internal liner. The doors are full height and clad to match.
- 3.7 The submitted Design & Access Statement sets out the following points in support of the application:
- “Since the South London Waste Partnership has brought in fortnightly collections across the Borough, this has impacted on waste storage to the blocks of flats on Carters Estate, resulting in a shortage of refuse storage on the site. The capacity has found to be too small to meet Merton’s requirements. In relation to this the designer has looked into various options and have considered the following in the layout and design for the bins stores:

- Most efficient way of distributing the locations of bins stores, which will have less impact to the residents and existing structures;
 - Ease of access by council refuse lorries;
 - Minimum effect to existing parking provisions to the whole estate.
- Because of the limitations of space, the option agreed was stand-alone bin enclosures located as near to the blocks as possible which has meant taking up several parking spaces across the estate. The alternative was to take out brick planted areas in these locations but the amount of groundwork involved was cost prohibitive. Having the proposed bin enclosures will allow for larger refuse and recycling facilities with greater capacity to cope with the reduced collection frequencies and will also mean that they comply with Clarion's fire safety recommendations.
 - In order to ensure sufficient provision of waste and recycling refuse areas, stores to house a total of 240 bins will be installed. The proposed increase in refuse provision will reduce the number of parking spaces throughout the site.
 - The current location of the existing bins is unsightly and pose a significant fire risk. Clarion have been advised that they need to relocate the refuse bins from inside the blocks to a location that is outside and away from the front of the building
 - This application seeks to address this urgent need with the provision of bin stores which are aesthetically pleasing and compact and installed in convenient and safe locations for the benefit of the residents.
 - The stores will be located so as not to have unacceptable impact on the amenity of residents and will not obstruct any designated emergency access routes.
 - This proposal will mean Clarion Housing Group can satisfy the requirements of fire risk assessments and improve amenities for their residents."

3.8 The application is accompanied by the following key supporting documents:

- Design and Access Statement
- Email summarising number of parking spaces to be lost (dated 01/09/2021)

4. RELEVANT PLANNING HISTORY

4.1 A number of applications for individual properties but none directly relevant to this proposal.

5. CONSULTATION

5.1 Site notice posted, neighbouring properties notified. 11 representations have been received raising objection on the following grounds:

- Parking is already difficult.
- The loss of any spaces will present further difficulties in parking.
- Suggestion that a permit system be introduced if these changes are to proceed.
- External bin storage will encourage fly-tipping and attract vermin.
- The existing system should be continued but improved.
- Suggestion that raised planters or other green spaces around the estate be removed to make space for the bin store as they do not add much to the area and this would avoid losing parking spaces.
- Concerns that people will simply pile rubbish by the bins as the narrow apertures will be inconvenient.
- Some bins are located adjacent to residential properties and there would be noise and smell disturbance, in addition to the impact on outlook from living areas. Suggest bins be located much further away from the properties.
- There has been no discussion between Clarion and residents.
- Query what steps will be put in place to assist residents with limited mobility.
- Query if steps will be taken to ensure residents do not continue putting rubbish in internal bin cupboard areas.
- Concern that the bin storage proposed would not give sufficient capacity compared to the existing arrangements, particularly on Polesden Gardens, which would have less refuse storage space available than currently (45 black bins to reduce to 36)
- Query how the cost of the bins will be levied on residents.
- Concern that maximum drag distance of 25m would be exceeded.
- The biggest issue for the residents living in flats is that the black bins are housed internally within the block itself. These blocks were designed in times of weekly bin collections, but as soon as they changed to fortnightly, it became intolerable, especially for those living next to the bin rooms on the ground floor.
- It is clear that with fortnightly collections, the black bins represent a health hazard and should therefore be housed outside. This should be prioritised over the recycling bins for health and hygiene reasons.

5.2 1 representation of support has been received, supporting but also raising concern on the loss of parking and concerns over the bins smelling. They also query what would happen to the internal bin cupboards and suggest use as bike/buggy rooms.

Internal consultees

5.3 LBM Highways:

The applicant must ensure that no bin stores are constructed on or open over the public highway

5.4 LBM Transport Planner:

The submitted survey indicates the loss of 29 spaces overall in comparison to ratio of 0.92% (existing) to 0.85% of (proposed).

The reduction is unlikely to have a significant impact on the surrounding highway network.

Recommendation: Raise no objection.

5.5 LBM Waste Management:

There is no objection to the planned waste arrangement. Kindly accept this as an approval.

6. POLICY CONTEXT

6.1 National Planning Policy Framework (2021)

- 2. Achieving Sustainable development
- 8. Promoting healthy and safe communities
- 11. Making effective use of land
- 12. Achieving well-designed places
- 15. Conserving and enhancing the historic environment

6.2 London Plan (2021)

- D1 London's form, character and capacity for growth.
- D4 Delivering good design
- D5 Inclusive design
- D8 Public realm
- D12 Fire Safety
- D14 Noise
- T6 Car parking
- T7 Deliveries, servicing and construction
- SI 7 Reducing waste and supporting the circular economy
- T1 Strategic Approach to transport
- T2 Healthy Streets

6.3 Merton Core Planning Strategy (July 2011)

- CS2 Mitcham Sub-Area
- CS11 Infrastructure
- CS13 Open Space, Nature Conservation, Leisure and Culture
- CS14 Design
- CS15 Climate Change
- CS17 Waste Management
- CS18 Active Transport
- CS20 Parking, Servicing and Delivery

6.4 Sites and Policies Plan and Policies Map (July 2014)

- DM D1 Urban design and the public realm
- DM D2 Design considerations in all developments

DM O2	Nature Conservation, trees, hedges and landscape features
DM T1	Support for sustainable transport and active travel
DM T2	Transport impacts of development
DM T3	Car parking and servicing standards

6.5 Supplementary planning guidance.

London Sustainable Design and Construction - SPG 2014
 London Character and Context SPG - 2014
 Merton's Design SPG 2004
 London Borough of Merton Municipal Waste Management Strategy 2006 – 2021
 South London Waste Plan 2012
 Draft South London Waste Plan (2012-2036) (Initial consultation stage)
 LBM Waste and Recycling Storage Requirements (undated)

7. PLANNING CONSIDERATIONS

7.1 Principle of development

7.1.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

7.1.2 The National Planning Policy Framework 2021, London Plan 2021 policies and the Council's Core Strategy policy CS17 seek to increase recycling rates and ensure that well-designed waste storage facilities, that will include recycling, are incorporated for new development where appropriate.

7.1.3 The applicant argues that the existing bin storage on the site is problematic for a number of reasons.

- The spaces are not large enough to accommodate the amount of refuse required which has led to over filling and over-spill of rubbish. (Recent changes to the recycling and refuse collection arrangements in Merton mean more recycling containers are required than previously).
- The spaces are incorporated into the structure of existing residential buildings and fires have been an on-going concern.

7.1.4 A robust form of communal bin storage is, therefore, considered appropriate. However, unless communal bin storage is intensively monitored and regularly kept clean, it can be subject to waste spills and attract additional small-scale dumping / fly-tipping and vermin with negative visual amenity impacts. Therefore, the success of the scheme would depend heavily on rigorous management and maintenance.

7.1.5 The proposal is considered to be acceptable in principle subject to compliance with other Development Management policies.

7.1.6 The key issues will be the impact on the amenities of residents, the visual impact of the proposed bin stores, access considerations and parking/highway considerations.

7.2 Impact on visual amenity

7.2.1 The National Planning Policy Framework (NPPF) states that planning should always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings. London-wide planning policy advice in relation to design states that Local Authorities should seek to ensure that developments promote high quality inclusive design, enhance the public realm, and seek to ensure that development promotes world class architecture and design.

7.2.2 Policy DM D2 seeks to ensure a high quality of design in all development, which relates positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings and existing street patterns, historic context, urban layout and landscape features of the surrounding area. Core Planning Policy CS14 supports this SPP Policy.

7.2.3 It is noted that the opportunities for siting the proposed bin stores are restricted in terms of distances to dwellings and availability of space to accommodate the bin stores. In addition, landscaped raised beds would not be removed, thereby retaining some of the visual relief they provide. Therefore, it is primarily within rows of parking bays and areas on the street where informal car parking occurs, where new bin stores would be sited.

7.2.4 In general design and appearance terms, the bin stores proposed are considered to be acceptable. The bins would be substantial and would stand at 1.5m in height, however, they would be spaced out around the estate, largely in areas already used for parking.

7.2.5 Officers acknowledge that the bin stores would have some impact on the visual amenities of the street scene, however, this limited visual intrusion is considered to be outweighed by the need for robust bin storage around the estate.

7.2.6 Whilst the proposal results in some impact on the character of the area, on balance, the benefit of providing the bin stores is considered to outweigh this limited harm.

7.2.7 It is also of note that the site has experienced incidents of fly-tipping and that the bin stores are intended to reduce the occurrences of this, which would improve the character of the area. In order to ensure good practice in terms of the use of the bin stores, it is important to have clear signage and sufficient storage space. With no clear signage or instructions communal bin stores can be susceptible to mis-use and contamination of recycling streams.

7.2.8 Subject to clear indications for the use of the bins and proper on-going maintenance, there is no reason to indicate that the bin stores would suffer from mis-use or overflowing bins. Officers advise that detailed management measures are controlled by way of condition.

7.2.9 The proposed bins would have some impact on the character of the area in terms of increased street clutter but they would enable a reduction in the more ad hoc arrangement of wheelie bins and overall the impact on visual amenity is considered to be acceptable.

7.3 Impact on residential amenity and environmental impact

7.3.1 Policy DM D2 seeks to ensure that development does not adversely impact on the amenity of nearby residential properties and that the living conditions of existing and future occupiers are not unduly diminished.

7.3.2 Officers acknowledge that there are problems associated with communal bin storage as opposed to individual bin storage, due to a lack of individual responsibility for managing waste in communal waste areas.

7.3.3 In general, communal bin stores can often be poorly managed. Once a bin store starts to look uncared for, people often dump their waste either on the ground or in the wrong bins. Poorly maintained areas that smell, are contaminated by spills and generally not cleaned quickly can encourage anti-social behaviour and a lack of pride or care. It is important to maintain these areas to a high standard.

7.3.4 The management and maintenance of the bin storage facilities will be critical in ensuring the success of the scheme.

7.3.5 The application details the position of bin stores in order to demonstrate that access is possible, however, in order to ensure that the rubbish/recycling scheme performs highly officers recommend that a pre-commencement condition to secure a Refuse and Recycling Operational Waste Plan (RROWP) to secure details of mitigation and management measures to include the following:

- Details of the frequency of collections.
- Schedule of on-going maintenance and cleaning.
- Fully accessible multi-channel communications and signage to support management and encourage desired recycling behaviours
- Contractual agreements with residents that include clear obligations on management of waste and use of facilities.
- Facilities and systems that support the collection and reporting of waste management information to help identify and address performance issues.
- provide communications and signage that is easily understood by different nationalities with varying proficiency in the English language.

- 7.3.6 Contingency arrangements should be made in case the waste collector does not pick up waste, for example during the Christmas period. The developer should agree actual collection cycles and servicing arrangements with the waste collection authority as part of the condition discharge process.
- 7.3.7 In addition to effective on-going management and monitoring it will be necessary to ensure effective user engagement. Officers recommend a pre-commencement condition to secure a user engagement plan to cover the following matters:
- Users need to be clearly informed as to how to use the service that is provided. This includes what waste materials go where and how they should be presented. Instructions should be made available within the residential unit. Each time a new resident occupies a unit they should be provided with clear instructions and ideally a face to face induction. Depending on the waste management arrangements, user instructions may need to be tailored 'block by block' and include details of:
 - The location of bin store areas. (potentially including a map of the location of the bin store);
 - Materials that are accepted and not accepted in each type of bin;
 - Arrangements for depositing of any bulky waste.
 - Clear user instructions on the property website (if applicable);
 - Engagement by site management / facilities management staff.
 - Details of signage in and around the container storage areas and within residential buildings. As a minimum all signs should:
 - be constructed from a durable material such as metal or rigid plastic;
 - be clear and use icons and images rather than words (English may not be the first language for some residents);
 - be appropriately located on or above waste/recycling containers, on the door of a container storage area etc.;
 - include information about food waste.
- 7.3.8 The layout and design of the proposed bin stores has the potential to result in a more effective refuse and recycling management system on site than currently exists. However, in order to ensure that the facility operates effectively officers recommend that details of the on-going management be secured by way of condition.
- 7.3.9 It is noted that the proposals do not include controlled access to the bin stores. Controlled access can be useful in that it ensures access to the bin stores is by residents only. However, given the inherent difficulties in the practicalities of this arrangement, which include on-going management, provision of keys or fobs where it may not be realistic to assume that keys or codes would be reliably

carried, it is considered that controlled access may not be beneficial to the effectiveness of the scheme.

7.3.10 It is noted that there is a level of objection to the proposed bin store arrangements. A large proportion of the objection letters cite concerns with the management of the bin stores and the potential for fly-tipping, vermin and other environmental issues that can be associated with communal bin storage. These concerns have been carefully considered and it is concluded that the effective management of the bin stores is critical to the success of the project. As set out above, a management program is intended to be secured by way of condition.

7.3.11 The management details will also be required to address what additional assistance will be provided for those with mobility issues.

7.3.12 The applicant has sought to ensure that bin drag distances are minimised and the proposals have been formulated in tandem with the Council's Waste Management section. Therefore, whilst there may be some areas where drag distances are marginally higher, the overall layout would be serviceable by the Council's Waste collection operatives.

7.3.13 In terms of fire safety, the proposals have been designed to improve fire safety by removing bin storage from within the residential buildings. Confederation of Fire Protection Associations in Europe (CFPA E) guidance specifically deals with 'Safety distances between waste containers and buildings'. The applicant has set out that the proposal would meet the relevant guidelines by being set at least 2.5m from any building opening. Whilst this matter would primarily be dealt with at the building control stage, officers note that Fire Safety CFPA-E guidance states that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. Therefore, in addition to any controls at the Building Control stage, officers recommend a pre-commencement condition requiring confirmation from a suitably qualified fire expert that the bins meet the required standards.

7.3.14 Subject to condition, no overriding concern is raised in relation to the proposals in terms of the impact on neighbouring amenity, environmental impacts of fire safety.

7.4 Transport, highway network and parking

7.4.1 Transport policies in the London Plan states that Development proposals should facilitate safe, clean, and efficient deliveries and servicing. Provision of adequate space for servicing, storage and deliveries should be made off-street, with on-street loading bays only used where this is not possible. Policy CS20 of the Core Planning Strategy seeks to implement effective traffic management by:

- Prioritising for people with restricted mobility and protecting vulnerable road users,
- Requiring developers to demonstrate that their development will not adversely affect pedestrian and cycle movements, safety, the

convenience of local residents or the quality of bus movement and/or facilities; on-street parking and traffic management,

- Requiring developers to incorporate adequate facilities for servicing to ensure loading and unloading activities do not have an adverse impact on the public highway
- Requiring developments to incorporate safe access to and from the public highway as well as on-site parking and manoeuvring for emergency vehicles, refuse storage and collection, and for service and delivery vehicles

7.4.2 The siting of the proposed bin stores is such that it would not have an adverse impact on refuse vehicle movements and the stores can be adequately accessed and no objection is raised in this regard.

7.4.3 The proposal would result in a reduction in car parking spaces across the site. In terms of planning policy, this reduction in parking levels is not objectionable as Transport for London guidance seeks to promote alternative modes of transport and therefore car parking standards are expressed as a maximum.

7.4.4 Maximum parking standards for an area with a PTAL of 2-3 have a maximum parking standard of 0.75 - 1 spaces per dwelling, in areas with a PTAL of 0-1 the maximum parking standard is 1.5 spaces per unit. The proposal would reduce the overall level of car parking from 0.92 spaces per dwelling to 0.85 spaces per dwelling.

7.4.5 It is noted that the site has a relatively low PTAL. However, the overall reduction in parking spaces would not warrant a refusal in planning policy terms. It is noted that the Council's Transport planner does not raise objection to the reduction in parking levels.

7.4.6 Notwithstanding this, it is noted that a number of objections have been received raising parking problems as a concern. This is noted and officers are aware that the estate can be used for commuter parking, which can exacerbate the issue. It is possible for residents of the estate to lobby the Council to create a Controlled Parking Zone but a need for one has not been identified by the Council's Transport Planner as a result of this application. Whilst it is appreciated that residents may consider that any reduction in parking provision would not be advantageous, in terms of planning policies, which seek to reduce reliance on private motor cars, officers conclude that there would not be a sufficient justification to warrant a refusal of planning permission.

7.4.8 The bin stores would be accessed from the top by residents. Waste collection operatives would use a door which would open over highway land. However, as this would be locked and restricted to waste collection operatives only, the Council's Highway Officer has confirmed that this arrangement is not objectionable.

7.4.7 The proposal is considered to be acceptable in terms of highway safety, capacity and in relation to parking considerations.

8. Conclusion

- 8.1 The requirement for recycling and food waste disposal through the Council's recent changes to refuse collection are such that the continued use of individual bin stores across the estate would exacerbate an existing problem with bin blight. Therefore, the need for a comprehensive refuse/recycling strategy is considered reasonable.
- 8.2 The overall benefit to the appearance and function of the estate, along with the actual benefits of increasing recycling rates are such that on balance, officers recommend that permission be granted. Given the degree to which Clarion can manage communal facilities such as this across the estate, the application of suitable safeguarding conditions relating to the management of the bin stores is considered to be pragmatic and enforceable.

9.0 Recommendation:

Grant planning permission subject to the following conditions:

1. Time limit
2. Approved Plans
3. Non Standard Condition – Fire resistance rating of bins.
4. Non Standard Condition – Management program for use of bin stores (including collection timetables, maintenance, cleaning, assistance for those with limited mobility and signage etc.)
5. H14 Doors/Gates – not to open over highway land (other than for collection operatives)
6. Non Standard Condition – Scheme to make good existing recessed bin stores
7. D11 Construction Times

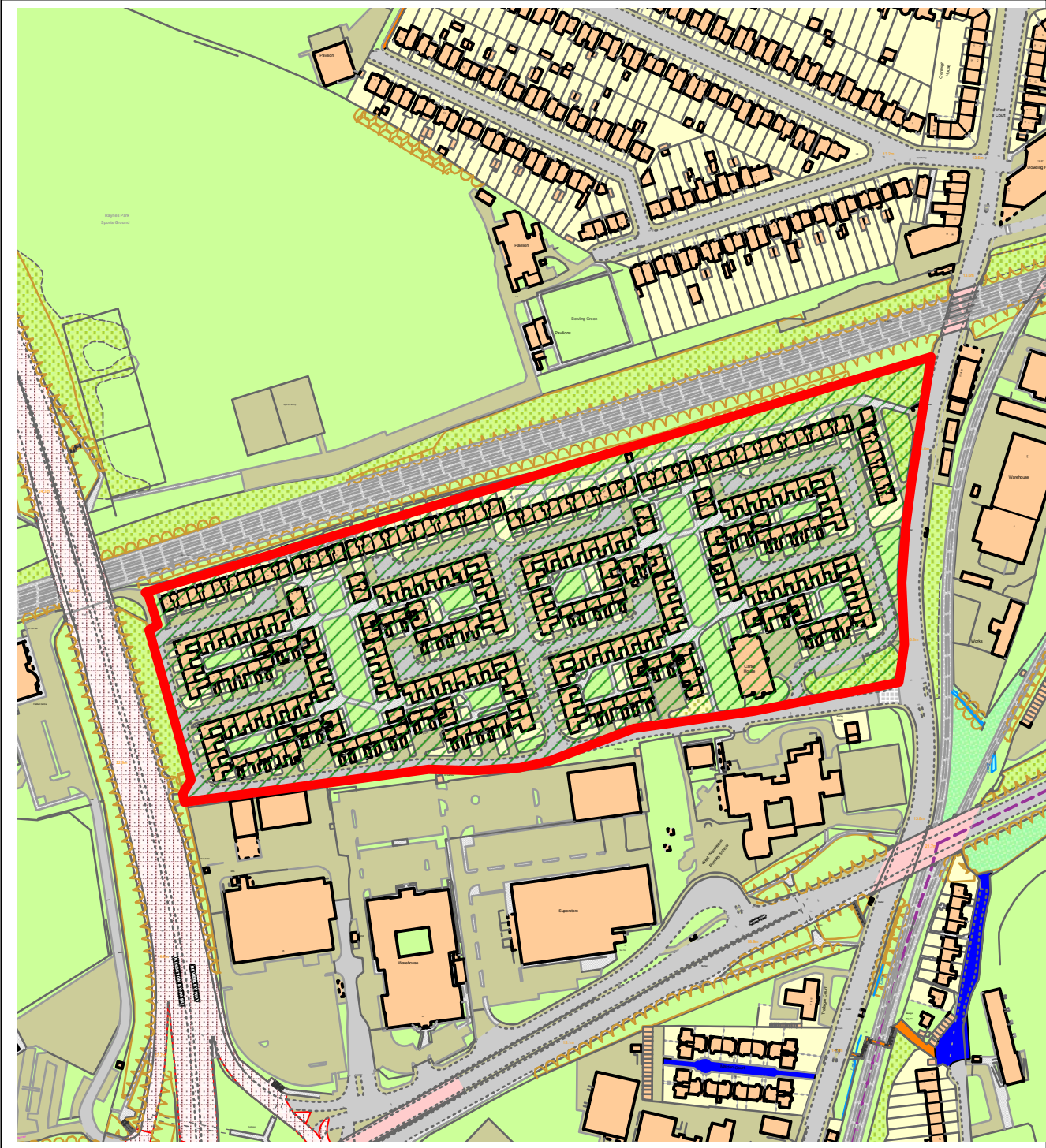
Informatives:

1. INF 09 Works on the Public Highway – You are advised to contact the Council's Highways team on 020 8545 3700 before undertaking any works within the Public Highway to obtain the necessary approvals and/or licences. Please be advised that there is a further charge for this work. If your application falls within a Controlled Parking Zone this has further costs involved and can delay the application by 6 to 12 months.
2. INF 12 Works affecting the public highway - Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or

affecting the public highway, shall be co-ordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Merton. Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with the London Borough of Merton, Network Coordinator, (telephone 020 8545 3976). This must take place at least one month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are co-ordinated to take place wherever possible at the same time.

3. INF 15 Discharge conditions prior to commencement of work
4. NPPF Informative – approved schemes

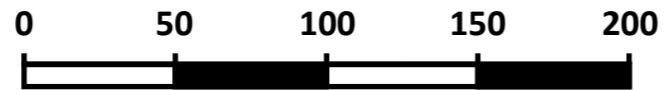
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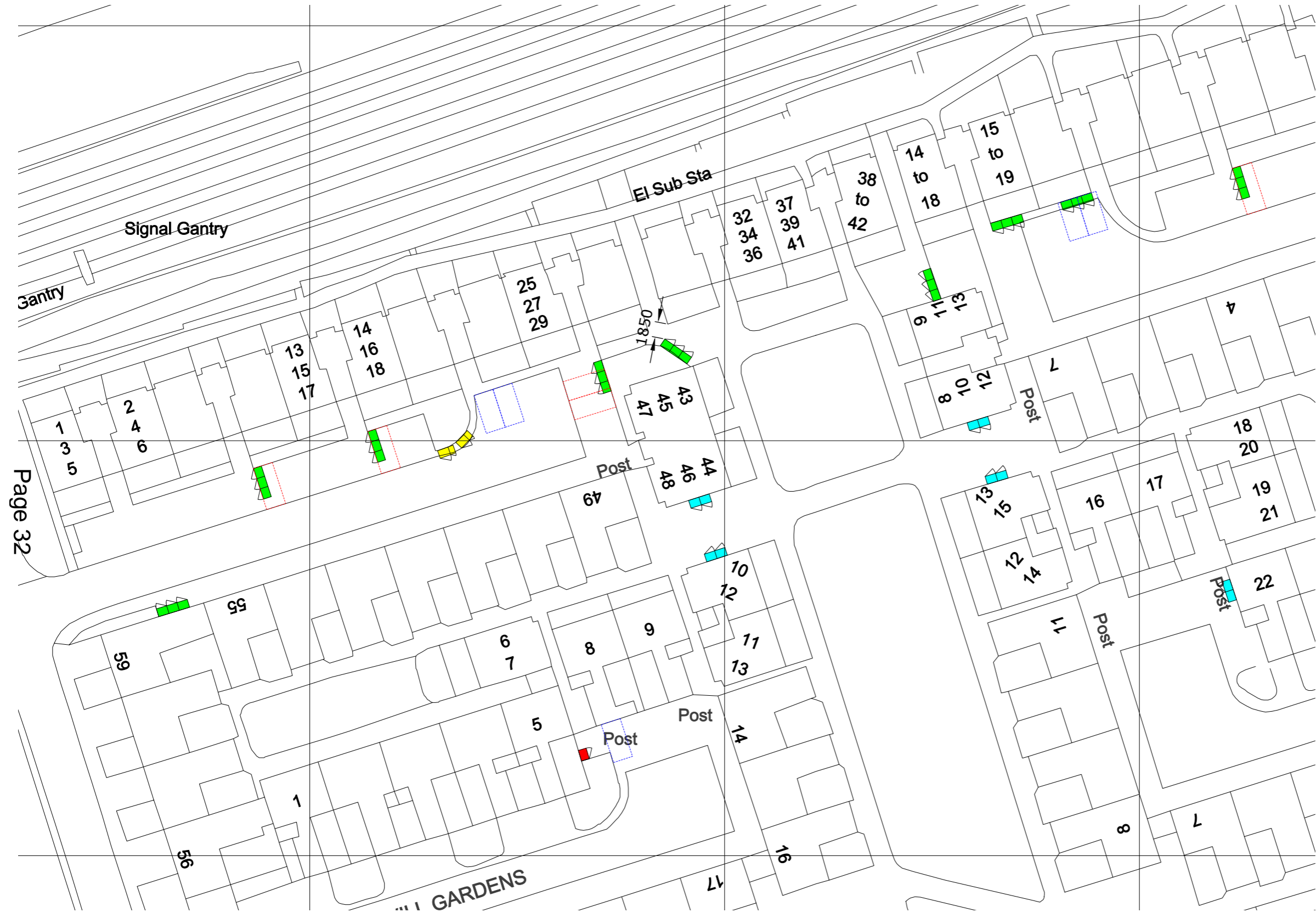


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REVISION

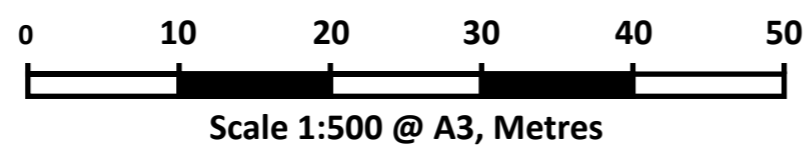
SHEET 1 OF 1

Block Plan 1-Proposed



- Key:**
- PBLX-2
 - PBLX-3
 - PBLX-4
 - PBLX-6
 - ▽ Door Swing
 - - - - - Parking space to be lost - Marked Bay
 - - - - - Parking space to be lost - Informal On-Street Space

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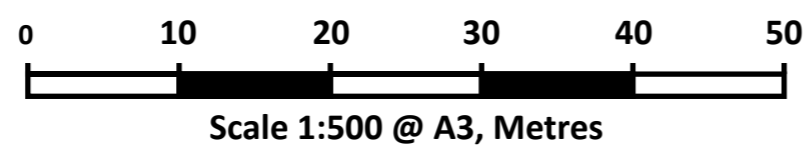


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SHEET 1 OF 1	



- Key:**
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 - PBLX-3
 - PBLX-4
 - PBLX-6
 - Door Swing
 - Parking space to be lost - Marked Bay
 - Parking space to be lost - Informal On-Street Space

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 outdoor storage systems	
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SHEET 1 OF 1	

Block Plan 3-Proposed

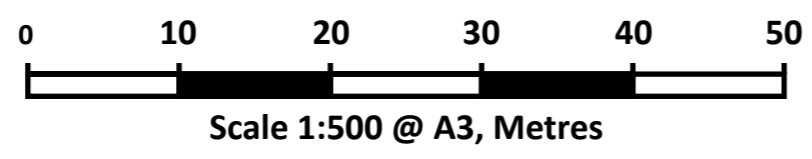


Key:

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- PBLX-3
- PBLX-4
- PBLX-6
- ▽ Door Swing
- ▭ Parking space to be lost - Marked Bay
- ▭ Parking space to be lost - Informal On-Street Space

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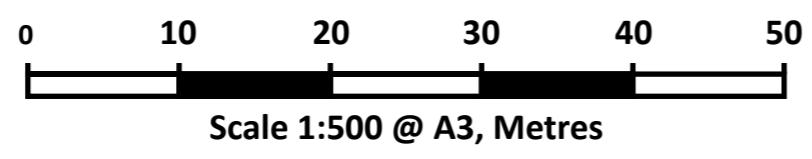
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DWG NO. CHG-CE-P-BP3-P	REVISION B
SHEET 1 OF 1	



Block Plan 4-Proposed



- Key:**
- PBLX-2
 - PBLX-3
 - PBLX-4
 - PBLX-6
 - ▽ Door Swing
 - ▭ Parking space to be lost - Marked Bay
 - ▭ Parking space to be lost - Informal On-Street Space



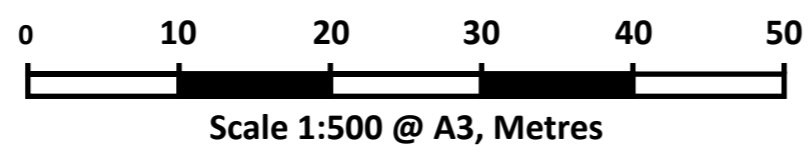
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SHEET 1 OF 1	

Block Plan 5-Proposed



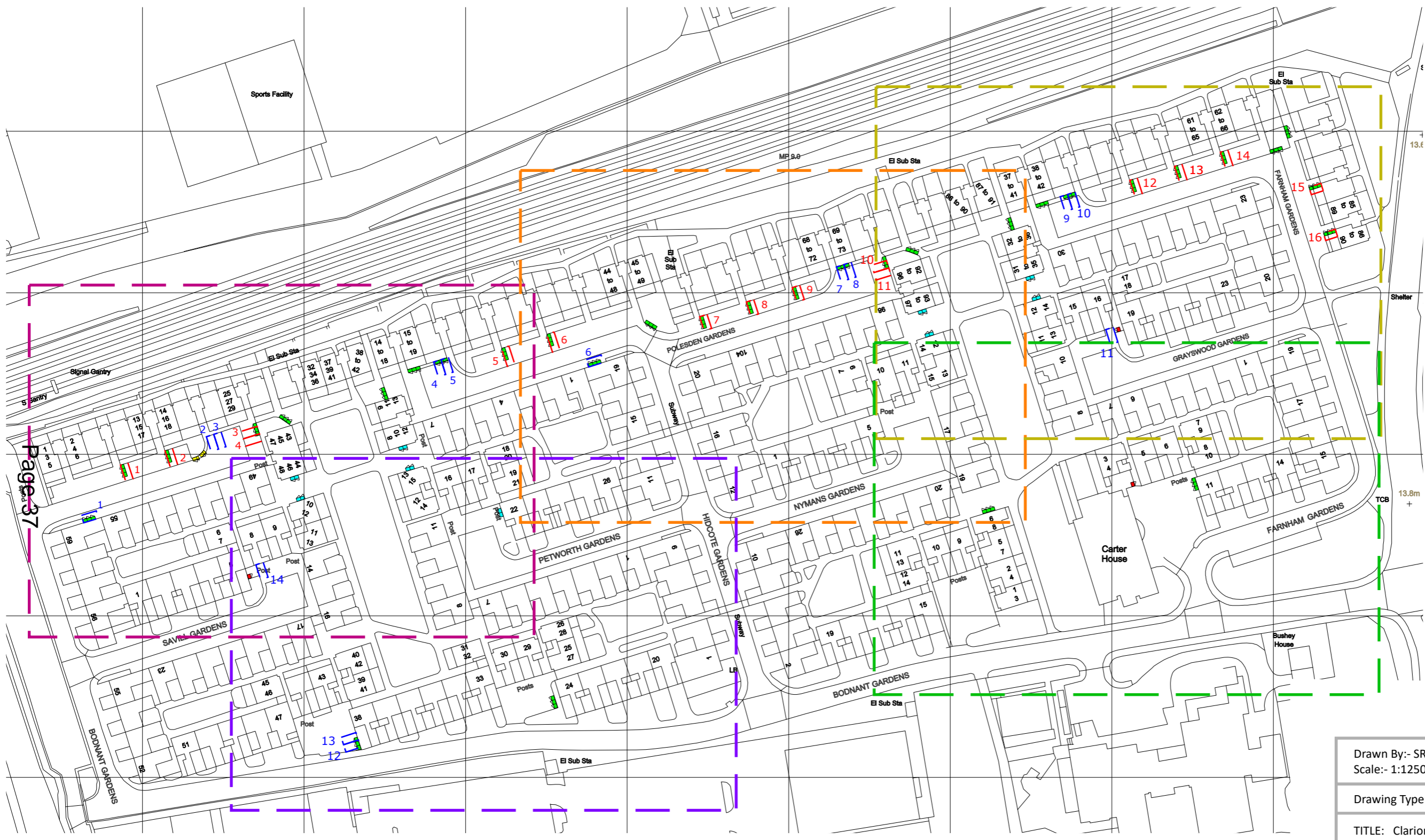
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 - PBLX-3
 - PBLX-4
 - PBLX-6
 - Door Swing
 - Parking space to be lost - Marked Bay
 - Parking space to be lost - Informal On-Street Space

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SHEET 1 OF 1	

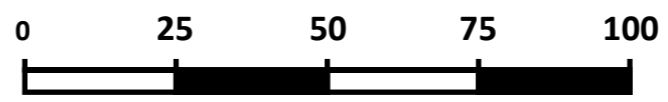
Block Plan Overview-Proposed



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Key:

- Block Plan 1
- Block Plan 2
- Block Plan 3
- Block Plan 4
- Block Plan 5
- Parking space to be lost - Marked Bay
- Parking space to be lost - Informal On-Street Space



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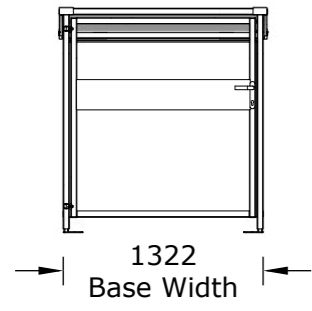


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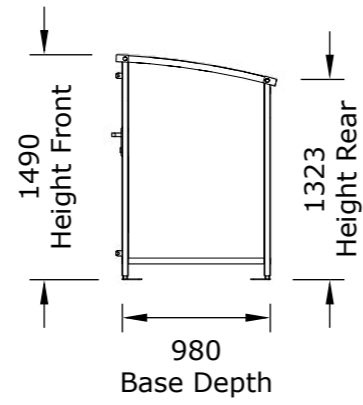


DWG NO. CHG-CE-P-BPO-P	REVISION B
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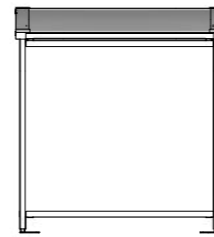
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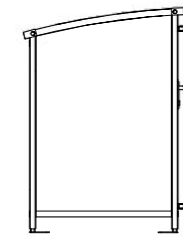
Right Elevation



Rear Elevation



Right Elevation



Plan View



Page 38



Scale 1:50 @ A3, Metres

Drawn By:- SRC
Scale:- 1:50 @ A3

Drawing Type:- PLANNING

TITLE: PBLX-2

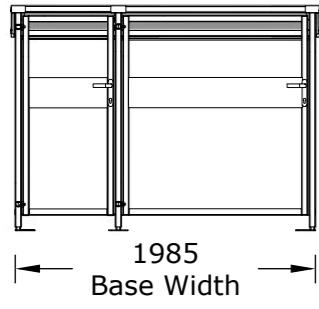
metroSTOR[®]
outdoor storage systems

DWG NO.
PBLX-2

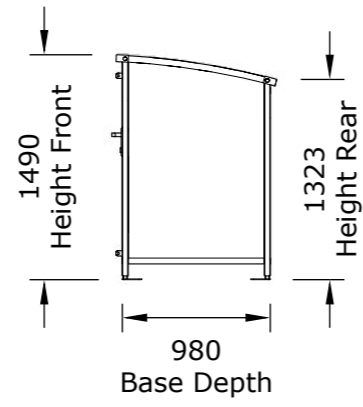
REVISION

SHEET 1 OF 1

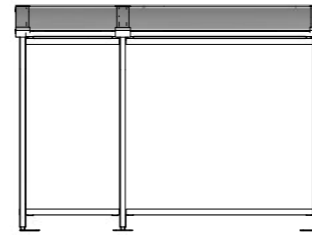
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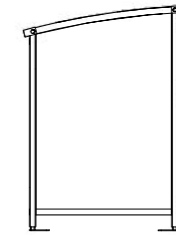
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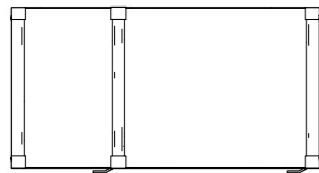
Rear Elevation



Right Elevation



Plan View



Scale 1:50 @ A3, Metres

Drawn By:- SRC
Scale:- 1:50 @ A3

Drawing Type:- PLANNING

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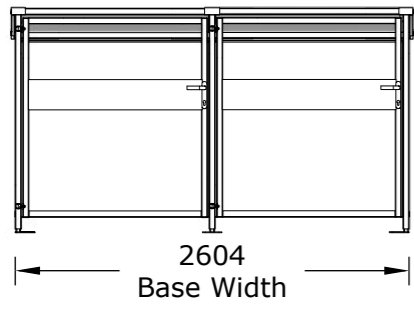
metroSTOR[®]
outdoor storage systems

DWG NO.
PBLX-3

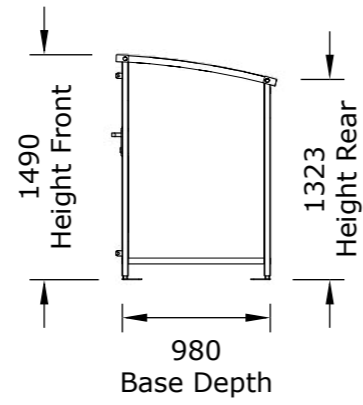
REVISION

SHEET 1 OF 1

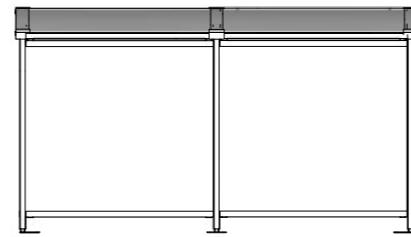
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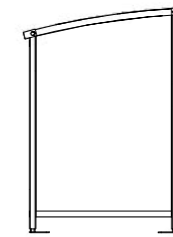
Right Elevation



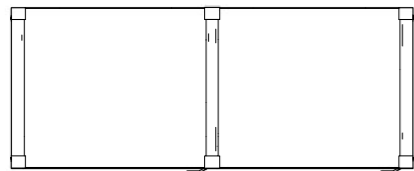
Rear Elevation



Right Elevation



Plan View



Page 40



Scale 1:50 @ A3, Metres

Drawn By:- SRC
Scale:- 1:50 @ A3

Drawing Type:- PLANNING

TITLE: PBLX-4

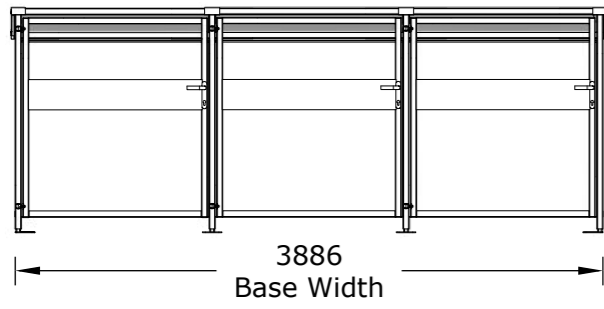
metroSTOR[®]
outdoor storage systems

DWG NO.
PBLX-4

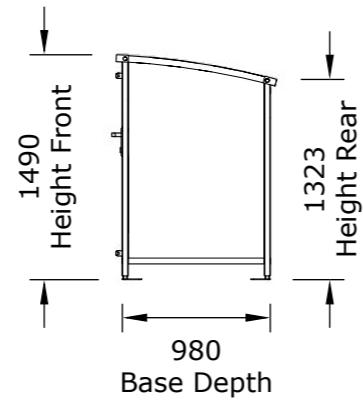
REVISION

SHEET 1 OF 1

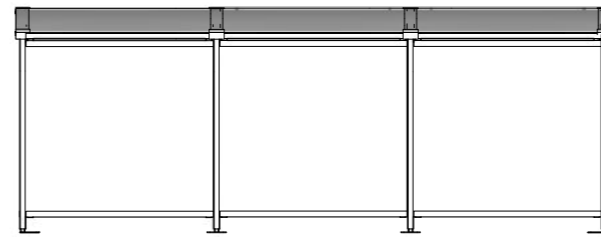
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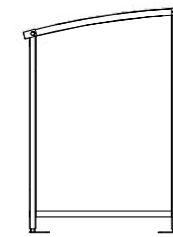
Right Elevation



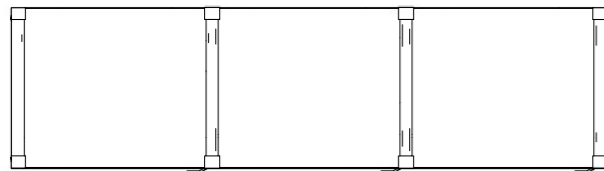
Rear Elevation



Right Elevation



Plan View



Page 41



Scale 1:50 @ A3, Metres

Drawn By:- SRC
Scale:- 1:50 @ A3

Drawing Type:- PLANNING

TITLE: PBLX-6

metroSTOR[®]
outdoor storage systems

DWG NO.
PBLX-6

REVISION

SHEET 1 OF 1

metroSTOR PBLX3

Bin Storage unit for 3no. 140L-360L Waste/Recycling Bins



metroSTOR Bin Stores

Refuse bin storage reinvented; metroSTOR products reduce fire risk, increase recycling rates and help eliminate cross contamination while raising the profile of local communities with reduced litter, fly-tipping and anti-social behaviour. The knowledge and experience from over 10,000 metroSTOR installations has created the only dedicated bin enclosure range in the world.

metroSTOR PBLX Bin Storage

Designed for residential applications where 2-wheeled bins are in use by individual dwellings, metroSTOR PBL is manufactured in 3 product versions accommodating all types from 140L – 360L capacity. metroSTOR PBLN is designed for 140L bins, metroSTOR PBL stores 140L to 240L types while metroSTOR PBLX accommodates the deeper 360L bin model. Specific internal unit dimensions ensure that bin lids cannot be left open with the resultant wind-blown litter issues and the availability of factory or retro-fitted recycling apertures helps prevent waste stream contamination. Optional integrated shelving enables kerbside and caddy type stacking bins to be accommodated within the unit.

A wide choice of cladding types are available within the standard frame design from traditional pressure treated softwood slats, recycled HDPE for maximum durability in 3 colours to our coated steel cassette panel system available in 4 colours and with an optional fireboard liner. Specific recycling streams are provided for with a coordinated aperture and signage system either factory assembled or retro fitted to suit changing site requirements.

One of the biggest risks from residential fires involving waste is unsecured bins. Waste materials catch alight easily, burn fiercely and generate large volumes of toxic smoke, with fires able to spread very quickly into adjacent dwellings if adequate precautions are not taken. As a consequence, Fire Prevention bodies such as the FPA and CFPA-E state that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. They should also be secured in place to prevent an arsonist from moving them closer to the building. metroSTOR PBL provides the safe solution for these critical challenges and can be specified with cladding providing 30 minute fire-resistance to resolve those situations where a 6m clearance cannot be achieved.

- 

Fire Safety
Fire Retardant cladding options for BS EN 13501 / BS476 Compliance where 6m building clearance cannot be achieved.
- 

Increased Recycling
Compact, low profile and easy to use communal recycling facilities for dense urban environments.
- 

Waste Stream Contamination Avoidance
Highly durable, easy to use bin loading apertures and signage for all types of waste and recycling streams.
- 

Reduced Side Waste & Fly Tipping
Bins are stored secure in the metroSTOR unit with lid open to ease loading and a bulky waste specification is available.
- 

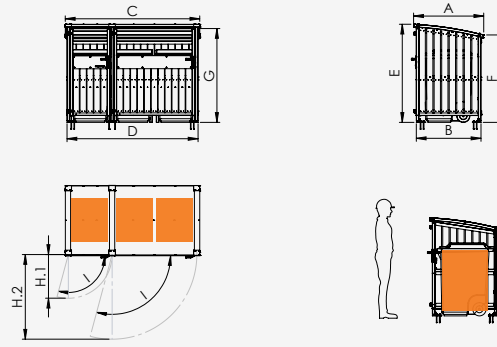
Accessible Bin Store Facilities
Aperture heights have been carefully designed to provide for the needs of all residents, including wheelchair-users.
- 

Increased Biodiversity
metroSTOR PBLX can be specified with WILD® Greenroof option creating valuable and enriching pockets of biodiversity.
- 

CAD Design Enabled
All metroSTOR PBLX unit variations are available for download as CAD blocks and BIM models.

Product Dimensions

(A) Roof Depth:	1062mm
(B) Base Depth:	980mm
(C) Roof Width:	2037mm
(D) Base Width:	1985mm
(E) Height Front:	1490mm
(F) Height Rear:	1323mm
(G) Door Height:	1316mm
(H.1) Single Door Clearance:	641mm
(H.2) Double Door Clearance:	1262mm
(I) Door Swing:	105°
Storage:	3no. 140L-360L Bins



PLEASE NOTE: Product dimensions given are correct at time of publishing December 2019. All dimensions are in millimetres and capacities in litres and don't form any part of the contract.

Product Features

- Unit Installation** metroSTOR modular units and unique base plinth design enables non-invasive installation on finished surfaces.
- Durable Build** metroSTOR products are UK manufactured in accordance with ISO9001:2008, ISO14001 processes. The all-steel frame is Hot-Dip Galvanised to BS EN 1461.
- Access Control** An integrated lock case within unit door frame enables slam latch and latch deadlock with key management and keypad release options.
- Heavy Duty Components** Fully welded, all steel frame manufacture with exhaustive testing of all components for harsh urban environments.

Cladding Types

ForestPanel™ <ul style="list-style-type: none"> FSC European Redwood 100mm PSE Plank 	ForestPanel™ TG FR <ul style="list-style-type: none"> FSC European Redwood 125mm TGV Plank Fireboard MgO Core Internal Liner 	ForestPanel™ Colour <ul style="list-style-type: none"> FSC European Redwood 100mm PSE Plank Coloured Woodstain System 	ForestPanel™ Cedar <ul style="list-style-type: none"> FSC Western Red Cedar 100mm PSE Plank Natural Finish 	ForestPanel™ Iroko <ul style="list-style-type: none"> FSC Iroko 70mm PSE Plank Oiled Finish 	Tuffplas™ Black <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Black Woodgrain Finish 	Tuffplas™ Brown <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Brown Woodgrain Finish
ForestPanel™ FR <ul style="list-style-type: none"> Adds Fire Retardant Coating 						
Tuffplas™ Grey <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Grey Woodgrain Finish 	Tuffplas™ Green <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Green Woodgrain Finish 	Firenze™ Freestyle <ul style="list-style-type: none"> Custom Design Perforated Pattern GreenCoat Pural BT Steel 	Firenze™ Design Wrap <ul style="list-style-type: none"> Galvanised Steel Panel SAV Graphic Wrap Finish Custom Design 	Firenze™ Black <ul style="list-style-type: none"> SSAB Nordic Night Black GreenCoat Pural BT Steel 	Firenze™ Brown <ul style="list-style-type: none"> SSAB Walnut Brown GreenCoat Pural BT Steel 	Firenze™ Anthracite <ul style="list-style-type: none"> SSAB Anthracite Grey GreenCoat Pural BT Steel
			Firenze™ Design Wrap FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	Firenze™ Black FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	Firenze™ Brown FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	Firenze™ Anthracite FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner
Firenze™ Green <ul style="list-style-type: none"> SSAB Leaf Green GreenCoat Pural BT Steel 	Firenze™ Grey <ul style="list-style-type: none"> SSAB Pebble Grey GreenCoat Pural BT Steel 	Firenze™ Blue <ul style="list-style-type: none"> SSAB Lake Blue GreenCoat Pural BT Steel 				
Firenze™ Green FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	Firenze™ Grey FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	Firenze™ Blue FR <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 				

Fire Safety

CFPA-E guidance states that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. ForestPanel™ TG FR and Firenze™ FR cladding options provide 30 minutes fire resistance so can be located within this safety distance, but any apertures breach this protection so should be facing away from the dwelling or replaced with a solid door. ForestPanel™ FR limits the materials reaction to fire but being an open slat design does not provide any resistance to penetration and should not be sited within 6m of a dwelling

Access Control Types



LS40 Slam Lock

- Self-Latching Daybolt
- FB2 Lever Key Release
- No Bolt Lock
- S.Steel Fixed Handle



LSE40 Latch + Eurocylinder Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Eurocylinder Key-operated Bolt Lock



LST40 Latch + Triangle Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Triangle Key-operated Bolt Lock



LSF40 Latch + FB2 Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- FB2 Lever Key-operated Self-Latching Bolt Lock



LSC40 Latch + Code Secure Lock

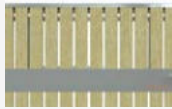
- Self-Latching Daybolt
- Mechanical Code Lock Release
- Aluminium Handle Daybolt Release



LSB40 Slide Bolt + Padlock

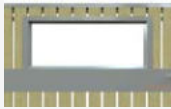
- No Latch
- Integral Slide Bolt / Handle with Padlock Provision
- Galvanised Steel Assembly

Waste & Recycling Type Aperture Options



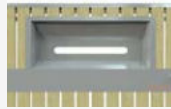
Solid Door – No Aperture

- Warden Controlled / Bulky Waste Applications



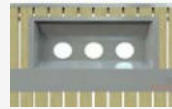
General Waste

- Allows Bagged Waste or Recycling



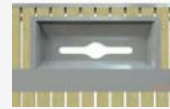
Cardboard Only

- Allows Only Non-Bagged Cardboard



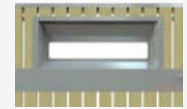
Bottle Only

- Allows Only Non-Bagged Bottles



Mixed Recycling

- Allows Only Non-Bagged Recycling



Signage Specified Recycling

- Allows Bagged and Non-Bagged Recycling

Waste & Recycling Stream Signage



Bulky Waste



General Waste



Non-Recyclables



Recycling



Food Waste



Mixed Waste



Paper



Paper & Cardboard



Cardboard



Mixed Paper



Cardboard & Cartons



Plastics



Plastics & Cans



Household Plastic Packaging



Glass



Mixed Glass Bottles & Jars



Plastics & Glass



Food Tins & Drink Cans



Cans, Bottles & Glass



Clinical Waste Only

metroSTOR PBLX can be fitted with a range of full colour exterior grade latex prints using eco-friendly solvent-free water based inks with a gloss protective overlamine. To ensure the artwork is not compromised by harsh urban environments the 530mm (W) x 160mm (H) prints are mounted onto 3mm thick aluminium composite panels for increased durability.

Please contact our sales team for further information regarding bespoke sizing and artwork.

Example Bespoke Signage



metroSTOR PBLX4

Bin Storage unit for 4no. 140L-360L
Waste/Recycling Bins



metroSTOR Bin Stores

Refuse bin storage reinvented; metroSTOR products reduce fire risk, increase recycling rates and help eliminate cross contamination while raising the profile of local communities with reduced litter, fly-tipping and anti-social behaviour. The knowledge and experience from over 10,000 metroSTOR installations has created the only dedicated bin enclosure range in the world.

metroSTOR PBLX Bin Storage

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A wide choice of cladding types are available within the standard frame design from traditional pressure treated softwood slats, recycled HDPE for maximum durability in 3 colours to our coated steel cassette panel system available in 4 colours and with an optional fireboard liner. Specific recycling streams are provided for with a coordinated aperture and signage system either factory assembled or retro fitted to suit changing site requirements.

One of the biggest risks from residential fires involving waste is unsecured bins. Waste materials catch alight easily, burn fiercely and generate large volumes of toxic smoke, with fires able to spread very quickly into adjacent dwellings if adequate precautions are not taken. As a consequence, Fire Prevention bodies such as the FPA and CFPA-E state that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. They should also be secured in place to prevent an arsonist from moving them closer to the building. metroSTOR PBL provides the safe solution for these critical challenges and can be specified with cladding providing 30 minute fire-resistance to resolve those situations where a 6m clearance cannot be achieved.



Fire Safety

Fire Retardant cladding options for BS EN 13501 / BS476 Compliance where 6m building clearance cannot be achieved.



Increased Recycling

Compact, low profile and easy to use communal recycling facilities for dense urban environments.



Waste Stream Contamination Avoidance

Highly durable, easy to use bin loading apertures and signage for all types of waste and recycling streams.



Reduced Side Waste & Fly Tipping

Bins are stored secure in the metroSTOR unit with lid open to ease loading and a bulky waste specification is available.



Accessible Bin Store Facilities

Aperture heights have been carefully designed to provide for the needs of all residents, including wheelchair-users.



Increased Biodiversity

metroSTOR PBLX can be specified with WILD® Greenroof option creating valuable and enriching pockets of biodiversity.

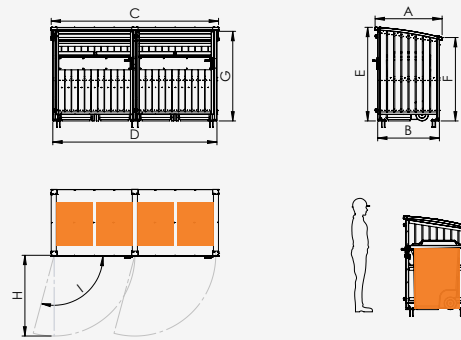


CAD Design Enabled

All metroSTOR PBLX unit variations are available for download as CAD blocks and BIM models.

Product Dimensions

(A) Roof Depth:	1062mm
(B) Base Depth:	980mm
(C) Roof Width:	2656mm
(D) Base Width:	2604mm
(E) Height Front:	1490mm
(F) Height Rear:	1323mm
(G) Door Height:	1316mm
(H) Door Clearance:	1262mm
(I) Door Swing:	105°
Storage:	4no. 140L-360L Bins
















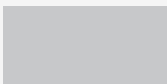



PLEASE NOTE: Product dimensions given are correct at time of publishing December 2019. All dimensions are in millimetres and capacities in litres and don't form any part of the contract.

Product Features

- Unit Installation** metroSTOR modular units and unique base plinth design enables non-invasive installation on finished surfaces.
- Durable Build** metroSTOR products are UK manufactured in accordance with ISO9001:2008, ISO14001 processes. The all-steel frame is Hot-Dip Galvanised to BS EN 1461.
- Access Control** An integrated lock case within unit door frame enables slam latch and latch deadlock with key management and keypad release options.
- Heavy Duty Components** Fully welded, all steel frame manufacture with exhaustive testing of all components for harsh urban environments.

Cladding Types

 <p>ForestPanel™</p> <ul style="list-style-type: none"> FSC European Redwood 100mm PSE Plank <p>ForestPanel™ FR</p> <ul style="list-style-type: none"> Adds Fire Retardant Coating 	 <p>ForestPanel™ TG FR</p> <ul style="list-style-type: none"> FSC European Redwood 125mm TGV Plank Fireboard MgO Core Internal Liner 	 <p>ForestPanel™ Colour</p> <ul style="list-style-type: none"> FSC European Redwood 100mm PSE Plank Coloured Woodstain System 	 <p>ForestPanel™ Cedar</p> <ul style="list-style-type: none"> FSC Western Red Cedar 100mm PSE Plank Natural Finish 	 <p>ForestPanel™ Iroko</p> <ul style="list-style-type: none"> FSC Iroko 70mm PSE Plank Oiled Finish 	 <p>Tuffplas™ Black</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Black Woodgrain Finish 	 <p>Tuffplas™ Brown</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Brown Woodgrain Finish
 <p>Tuffplas™ Grey</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Grey Woodgrain Finish 	 <p>Tuffplas™ Green</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Green Woodgrain Finish 	 <p>Firenze™ Freestyle</p> <ul style="list-style-type: none"> Custom Design Perforated Pattern GreenCoat Pural BT Steel 	 <p>Firenze™ Design Wrap</p> <ul style="list-style-type: none"> Galvanised Steel Panel SAV Graphic Wrap Finish Custom Design <p>Firenze™ Design Wrap FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Black</p> <ul style="list-style-type: none"> SSAB Nordic Night Black GreenCoat Pural BT Steel <p>Firenze™ Black FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Brown</p> <ul style="list-style-type: none"> SSAB Walnut Brown GreenCoat Pural BT Steel <p>Firenze™ Brown FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Anthracite</p> <ul style="list-style-type: none"> SSAB Anthracite Grey GreenCoat Pural BT Steel <p>Firenze™ Anthracite FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner
 <p>Firenze™ Green</p> <ul style="list-style-type: none"> SSAB Leaf Green GreenCoat Pural BT Steel <p>Firenze™ Green FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Grey</p> <ul style="list-style-type: none"> SSAB Pebble Grey GreenCoat Pural BT Steel <p>Firenze™ Grey FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Blue</p> <ul style="list-style-type: none"> SSAB Lake Blue GreenCoat Pural BT Steel <p>Firenze™ Blue FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 				

Fire Safety

CFPA-E guidance states that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. ForestPanel™ TG FR and Firenze™ FR cladding options provide 30 minutes fire resistance so can be located within this safety distance, but any apertures breach this protection so should be facing away from the dwelling or replaced with a solid door. ForestPanel™ FR limits the materials reaction to fire but being an open slat design does not provide any resistance to penetration and should not be sited within 6m of a dwelling

Access Control Types



LS40 Slam Lock

- Self-Latching Daybolt
- FB2 Lever Key Release
- No Bolt Lock
- S.Steel Fixed Handle



LSE40 Latch + Eurocylinder Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Eurocylinder Key-operated Bolt Lock



LST40 Latch + Triangle Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Triangle Key-operated Bolt Lock



LSF40 Latch + FB2 Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- FB2 Lever Key-operated Self-Latching Bolt Lock



LSC40 Latch + Code Secure Lock

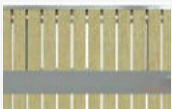
- Self-Latching Daybolt
- Mechanical Code Lock Release
- Aluminium Handle Daybolt Release



LSB40 Slide Bolt + Padlock

- No Latch
- Integral Slide Bolt / Handle with Padlock Provision
- Galvanised Steel Assembly

Waste & Recycling Type Aperture Options



Solid Door – No Aperture

- Warden Controlled / Bulky Waste Applications



General Waste

- Allows Bagged Waste or Recycling



Cardboard Only

- Allows Only Non-Bagged Cardboard



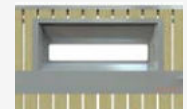
Bottle Only

- Allows Only Non-Bagged Bottles



Mixed Recycling

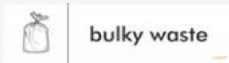
- Allows Only Non-Bagged Recycling



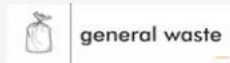
Signage Specified Recycling

- Allows Bagged and Non-Bagged Recycling

Waste & Recycling Stream Signage



Bulky Waste



General Waste



Non-Recyclables



Recycling



Food Waste



Mixed Waste



Paper



Paper & Cardboard



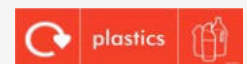
Cardboard



Mixed Paper



Cardboard & Cartons



Plastics



Plastics & Cans



Household Plastic Packaging



Glass



Mixed Glass Bottles & Jars



Plastics & Glass



Food Tins & Drink Cans



Cans, Bottles & Glass



Clinical Waste Only

metroSTOR PBLX can be fitted with a range of full colour exterior grade latex prints using eco-friendly solvent-free water based inks with a gloss protective overlamine. To ensure the artwork is not compromised by harsh urban environments the 530mm (W) x 160mm (H) prints are mounted onto 3mm thick aluminium composite panels for increased durability.

Please contact our sales team for further information regarding bespoke sizing and artwork.

Example Bespoke Signage



metroSTOR PBLX2

Bin Storage unit for 2no. 140L-360L
Waste/Recycling Bins



metroSTOR Bin Stores

Refuse bin storage reinvented; metroSTOR products reduce fire risk, increase recycling rates and help eliminate cross contamination while raising the profile of local communities with reduced litter, fly-tipping and anti-social behaviour. The knowledge and experience from over 10,000 metroSTOR installations has created the only dedicated bin enclosure range in the world.

metroSTOR PBLX Bin Storage

Designed for residential applications where 2-wheeled bins are in use by individual dwellings, metroSTOR PBL is manufactured in 3 product versions accommodating all types from 140L – 360L capacity. metroSTOR PBLN is designed for 140L bins, metroSTOR PBL stores 140L to 240L types while metroSTOR PBLX accommodates the deeper 360L bin model. Specific internal unit dimensions ensure that bin lids cannot be left open with the resultant wind-blown litter issues and the availability of factory or retro-fitted recycling apertures helps prevent waste stream contamination. Optional integrated shelving enables kerbside and caddy type stacking bins to be accommodated within the unit.

A wide choice of cladding types are available within the standard frame design from traditional pressure treated softwood slats, recycled HDPE for maximum durability in 3 colours to our coated steel cassette panel system available in 4 colours and with an optional fireboard liner. Specific recycling streams are provided for with a coordinated aperture and signage system either factory assembled or retro fitted to suit changing site requirements.

One of the biggest risks from residential fires involving waste is unsecured bins. Waste materials catch alight easily, burn fiercely and generate large volumes of toxic smoke, with fires able to spread very quickly into adjacent dwellings if adequate precautions are not taken. As a consequence, Fire Prevention bodies such as the FPA and CFPA-E state that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. They should also be secured in place to prevent an arsonist from moving them closer to the building. metroSTOR PBL provides the safe solution for these critical challenges and can be specified with cladding providing 30 minute fire-resistance to resolve those situations where a 6m clearance cannot be achieved.



Fire Safety

Fire Retardant cladding options for BS EN 13501 / BS476 Compliance where 6m building clearance cannot be achieved.



Increased Recycling

Compact, low profile and easy to use communal recycling facilities for dense urban environments.



Waste Stream Contamination Avoidance

Highly durable, easy to use bin loading apertures and signage for all types of waste and recycling streams.



Reduced Side Waste & Fly Tipping

Bins are stored secure in the metroSTOR unit with lid open to ease loading and a bulky waste specification is available.



Accessible Bin Store Facilities

Aperture heights have been carefully designed to provide for the needs of all residents, including wheelchair-users.



Increased Biodiversity

metroSTOR PBLX can be specified with WILD® Greenroof option creating valuable and enriching pockets of biodiversity.

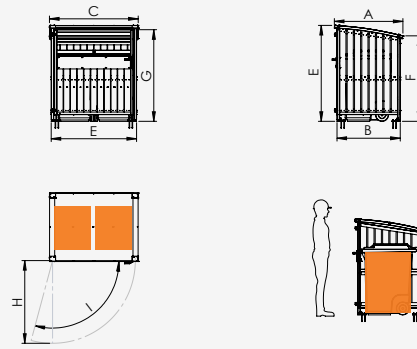


CAD Design Enabled

All metroSTOR PBLX unit variations are available for download as CAD blocks and BIM models.

Product Dimensions

(A) Roof Depth:	1062mm
(B) Base Depth:	980mm
(C) Roof Width:	1374mm
(D) Base Width:	1322mm
(E) Height Front:	1490mm
(F) Height Rear:	1323mm
(G) Door Height:	1316mm
(H) Door Clearance:	1262mm
(I) Door Swing:	105°
Storage:	2no. 140L-360L Bins
















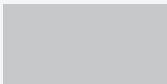



PLEASE NOTE: Product dimensions given are correct at time of publishing December 2019. All dimensions are in millimetres and capacities in litres and don't form any part of the contract.

Product Features

- Unit Installation** metroSTOR modular units and unique base plinth design enables non-invasive installation on finished surfaces.
- Durable Build** metroSTOR products are UK manufactured in accordance with ISO9001:2008, ISO14001 processes. The all-steel frame is Hot-Dip Galvanised to BS EN 1461.
- Access Control** An integrated lock case within unit door frame enables slam latch and latch deadlock with key management and keypad release options.
- Heavy Duty Components** Fully welded, all steel frame manufacture with exhaustive testing of all components for harsh urban environments.

Cladding Types

 <p>ForestPanel™</p> <ul style="list-style-type: none"> FSC European Redwood 100mm PSE Plank <p>ForestPanel™ FR</p> <ul style="list-style-type: none"> Adds Fire Retardant Coating 	 <p>ForestPanel™ TG FR</p> <ul style="list-style-type: none"> FSC European Redwood 125mm TGV Plank Fireboard MgO Core Internal Liner 	 <p>ForestPanel™ Colour</p> <ul style="list-style-type: none"> FSC European Redwood 100mm PSE Plank Coloured Woodstain System 	 <p>ForestPanel™ Cedar</p> <ul style="list-style-type: none"> FSC Western Red Cedar 100mm PSE Plank Natural Finish 	 <p>ForestPanel™ Iroko</p> <ul style="list-style-type: none"> FSC Iroko 70mm PSE Plank Oiled Finish 	 <p>Tuffplas™ Black</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Black Woodgrain Finish 	 <p>Tuffplas™ Brown</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Brown Woodgrain Finish
 <p>Tuffplas™ Grey</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Grey Woodgrain Finish 	 <p>Tuffplas™ Green</p> <ul style="list-style-type: none"> HDPE 100% Recycled 100mm SE Plank Green Woodgrain Finish 	 <p>Firenze™ Freestyle</p> <ul style="list-style-type: none"> Custom Design Perforated Pattern GreenCoat Pural BT Steel 	 <p>Firenze™ Design Wrap</p> <ul style="list-style-type: none"> Galvanised Steel Panel SAV Graphic Wrap Finish Custom Design <p>Firenze™ Design Wrap FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Black</p> <ul style="list-style-type: none"> SSAB Nordic Night Black GreenCoat Pural BT Steel <p>Firenze™ Black FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Brown</p> <ul style="list-style-type: none"> SSAB Walnut Brown GreenCoat Pural BT Steel <p>Firenze™ Brown FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Anthracite</p> <ul style="list-style-type: none"> SSAB Anthracite Grey GreenCoat Pural BT Steel <p>Firenze™ Anthracite FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner
 <p>Firenze™ Green</p> <ul style="list-style-type: none"> SSAB Leaf Green GreenCoat Pural BT Steel <p>Firenze™ Green FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Grey</p> <ul style="list-style-type: none"> SSAB Pebble Grey GreenCoat Pural BT Steel <p>Firenze™ Grey FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 	 <p>Firenze™ Blue</p> <ul style="list-style-type: none"> SSAB Lake Blue GreenCoat Pural BT Steel <p>Firenze™ Blue FR</p> <ul style="list-style-type: none"> Adds Fireboard MgO Core Internal Liner 				

Fire Safety

CFPA-E guidance states that bins should not be stored within 6m of a dwelling unless within a structure providing 30 minutes fire resistance. ForestPanel™ TG FR and Firenze™ FR cladding options provide 30 minutes fire resistance so can be located within this safety distance, but any apertures breach this protection so should be facing away from the dwelling or replaced with a solid door. ForestPanel™ FR limits the materials reaction to fire but being an open slat design does not provide any resistance to penetration and should not be sited within 6m of a dwelling

Access Control Types



LS40 Slam Lock

- Self-Latching Daybolt
- FB2 Lever Key Release
- No Bolt Lock
- S.Steel Fixed Handle



LSE40 Latch + Eurocylinder Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Eurocylinder Key-operated Bolt Lock



LST40 Latch + Triangle Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Triangle Key-operated Bolt Lock



LSF40 Latch + FB2 Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- FB2 Lever Key-operated Self-Latching Bolt Lock



LSC40 Latch + Code Secure Lock

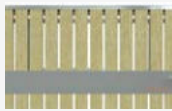
- Self-Latching Daybolt
- Mechanical Code Lock Release
- Aluminium Handle Daybolt Release



LSB40 Slide Bolt + Padlock

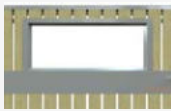
- No Latch
- Integral Slide Bolt / Handle with Padlock Provision
- Galvanised Steel Assembly

Waste & Recycling Type Aperture Options



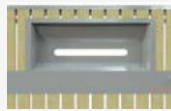
Solid Door – No Aperture

- Warden Controlled / Bulky Waste Applications



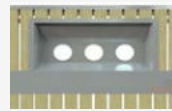
General Waste

- Allows Bagged Waste or Recycling



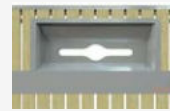
Cardboard Only

- Allows Only Non-Bagged Cardboard



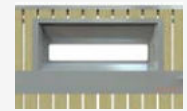
Bottle Only

- Allows Only Non-Bagged Bottles



Mixed Recycling

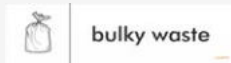
- Allows Only Non-Bagged Recycling



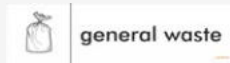
Signage Specified Recycling

- Allows Bagged and Non-Bagged Recycling

Waste & Recycling Stream Signage



Bulky Waste



General Waste



Non-Recyclables



Recycling



Food Waste



Mixed Waste



Paper



Paper & Cardboard



Cardboard



Mixed Paper



Cardboard & Cartons



Plastics



Plastics & Cans



Household Plastic Packaging



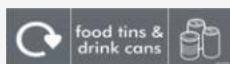
Glass



Mixed Glass Bottles & Jars



Plastics & Glass



Food Tins & Drink Cans



Cans, Bottles & Glass



Clinical Waste Only

metroSTOR PBLX can be fitted with a range of full colour exterior grade latex prints using eco-friendly solvent-free water based inks with a gloss protective overlamine. To ensure the artwork is not compromised by harsh urban environments the 530mm (W) x 160mm (H) prints are mounted onto 3mm thick aluminium composite panels for increased durability.

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Example Bespoke Signage



metroSTOR PBLX6

Bin Storage unit for 6no. 140L-360L Waste/Recycling Bins



metroSTOR Bin Stores

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Fire Safety

Fire Retardant cladding options for BS EN 13501 / BS476 Compliance where 6m building clearance cannot be achieved.



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Highly durable, easy to use bin loading apertures and signage for all types of waste and recycling streams.



Reduced Side Waste & Fly Tipping

Bins are stored secure in the metroSTOR unit with lid open to ease loading and a bulky waste specification is available.



Accessible Bin Store Facilities

Aperture heights have been carefully designed to provide for the needs of all residents, including wheelchair-users.



Increased Biodiversity

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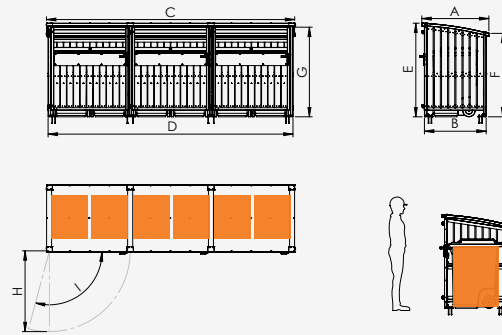


CAD Design Enabled

All metroSTOR PBLX unit variations are available for download as CAD blocks and BIM models.

Product Dimensions

(A) Roof Depth:	1062mm
(B) Base Depth:	980mm
(C) Roof Width:	3938mm
(D) Base Width:	3886mm
(E) Height Front:	1490mm
(F) Height Rear:	1323mm
(G) Door Height:	1316mm
(H) Door Clearance:	1262mm
(I) Door Swing:	105°
Storage:	6no. 140L-360L Bins



PLEASE NOTE: Product dimensions given are correct at time of publishing December 2019. All dimensions are in millimetres and capacities in litres and don't form any part of the contract.

Product Features

- Unit Installation** metroSTOR modular units and unique base plinth design enables non-invasive installation on finished surfaces.
- Durable Build** metroSTOR products are UK manufactured in accordance with ISO9001:2008, ISO14001 processes. The all-steel frame is Hot-Dip Galvanised to BS EN 1461.
- Access Control** An integrated lock case within unit door frame enables slam latch and latch deadlock with key management and keypad release options.
- Heavy Duty Components** Fully welded, all steel frame manufacture with exhaustive testing of all components for harsh urban environments.

Cladding Types



- ForestPanel™**
- FSC European Redwood
 - 100mm PSE Plank
- ForestPanel™ FR**
- Adds Fire Retardant Coating



- ForestPanel™ TG FR**
- FSC European Redwood
 - 125mm TGV Plank
 - Fireboard MgO Core Internal Liner



- ForestPanel™ Colour**
- FSC European Redwood
 - 100mm PSE Plank
 - Coloured Woodstain System



- ForestPanel™ Cedar**
- FSC Western Red Cedar
 - 100mm PSE Plank
 - Natural Finish



- ForestPanel™ Iroko**
- FSC Iroko
 - 70mm PSE Plank
 - Oiled Finish



- Tuffplas™ Black**
- HDPE 100% Recycled
 - 100mm SE Plank
 - Black Woodgrain Finish



- Tuffplas™ Brown**
- HDPE 100% Recycled
 - 100mm SE Plank
 - Brown Woodgrain Finish



- Tuffplas™ Grey**
- HDPE 100% Recycled
 - 100mm SE Plank
 - Grey Woodgrain Finish



- Tuffplas™ Green**
- HDPE 100% Recycled
 - 100mm SE Plank
 - Green Woodgrain Finish



- Firenze™ Freestyle**
- Custom Design
 - Perforated Pattern
 - GreenCoat Pural BT Steel



- Firenze™ Design Wrap**
- Galvanised Steel Panel
 - SAV Graphic Wrap Finish
 - Custom Design



- Firenze™ Black**
- SSAB Nordic Night Black
 - GreenCoat Pural BT Steel



- Firenze™ Brown**
- SSAB Walnut Brown
 - GreenCoat Pural BT Steel



- Firenze™ Anthracite**
- SSAB Anthracite Grey
 - GreenCoat Pural BT Steel

- Firenze™ Design Wrap FR**
- Adds Fireboard MgO Core Internal Liner

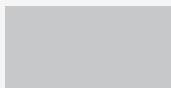
- Firenze™ Black FR**
- Adds Fireboard MgO Core Internal Liner

- Firenze™ Brown FR**
- Adds Fireboard MgO Core Internal Liner

- Firenze™ Anthracite FR**
- Adds Fireboard MgO Core Internal Liner



- Firenze™ Green**
- SSAB Leaf Green
 - GreenCoat Pural BT Steel



- Firenze™ Grey**
- SSAB Pebble Grey
 - GreenCoat Pural BT Steel



- Firenze™ Blue**
- SSAB Lake Blue
 - GreenCoat Pural BT Steel

- Firenze™ Green FR**
- Adds Fireboard MgO Core Internal Liner

- Firenze™ Grey FR**
- Adds Fireboard MgO Core Internal Liner

- Firenze™ Blue FR**
- Adds Fireboard MgO Core Internal Liner

Fire Safety

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Access Control Types



LS40 Slam Lock

- Self-Latching Daybolt
- FB2 Lever Key Release
- No Bolt Lock
- S.Steel Fixed Handle



LSE40 Latch + Eurocylinder Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Eurocylinder Key-operated Bolt Lock



LST40 Latch + Triangle Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- Triangle Key-operated Bolt Lock



LSF40 Latch + FB2 Key Lock

- Self-Latching Daybolt
- Aluminium Handle Release
- FB2 Lever Key-operated Self-Latching Bolt Lock



LSC40 Latch + Code Secure Lock

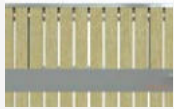
- Self-Latching Daybolt
- Mechanical Code Lock Release
- Aluminium Handle Daybolt Release



LSB40 Slide Bolt + Padlock

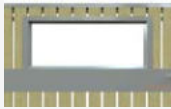
- No Latch
- Integral Slide Bolt / Handle with Padlock Provision
- Galvanised Steel Assembly

Waste & Recycling Type Aperture Options



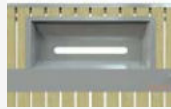
Solid Door - No Aperture

- Warden Controlled / Bulky Waste Applications



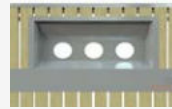
General Waste

- Allows Bagged Waste or Recycling



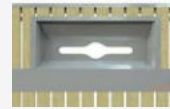
Cardboard Only

- Allows Only Non-Bagged Cardboard



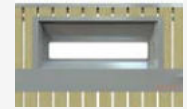
Bottle Only

- Allows Only Non-Bagged Bottles



Mixed Recycling

- Allows Only Non-Bagged Recycling



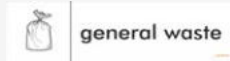
Signage Specified Recycling

- Allows Bagged and Non-Bagged Recycling

Waste & Recycling Stream Signage



Bulky Waste



General Waste



Non-Recyclables



Recycling



Food Waste



Mixed Waste



Paper



Paper & Cardboard



Cardboard



Mixed Paper



Cardboard & Cartons



Plastics



Plastics & Cans



Household Plastic Packaging



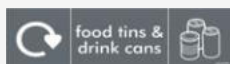
Glass



Mixed Glass Bottles & Jars



Plastics & Glass



Food Tins & Drink Cans



Cans, Bottles & Glass



Clinical Waste Only

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Please contact our sales team for further information regarding bespoke sizing and artwork.

Example Bespoke Signage



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PLANNING APPLICATIONS COMMITTEE
28th April 2022.

UPRN	APPLICATION NO.	Item No: DATE VALID
	20/P2583	17.08.2020
Address/Site	Electrical sub station adjacent to 14 Pepys Road Raynes Park SW20 8NH	
Ward:	Raynes Park	
Proposal:	Demolition of existing substation buildings and development of site to create 2 x 3 storey 3 bedroom dwellinghouses	
Drawing Nos	Site location plan and drawings 1141P- 20, 40, 200, 201, 210, 211, 212 & 213	
Contact Officer:	Leigh Harrington (020 8545 3836)	

RECOMMENDATION

Grant planning permission subject to conditions and s106 agreement.

CHECKLIST INFORMATION

- Heads of agreement: Permit Free
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No
- Design Review Panel consulted: No
- Number of neighbours consulted: 177
- Press notice – No
- Site notice – No
- External consultations: No
- Archaeological Priority Zone – No
- Controlled Parking Zone – Yes (RPE)
- Number of jobs created: N/A
- Density 260 Habitable rooms per ha.

1 INTRODUCTION

- 1.1 The application has been brought before the Planning Applications Committee for consideration as it falls outside of the Scheme of Delegation due to officer recommendation of grant permission subject to conditions and S106 agreement and the number of objections received from local residents against the application.

2. SITE AND SURROUNDINGS

- 2.1 The application site is a parcel of land positioned along the eastern side of Pepys Road within Raynes Park, located between no.14 Pepys Road and no. 264 Worple Road. The site sits at the junction corner, being fronted by a zebra pedestrian crossing and adjoins Langham Court at the rear. The land is narrow and elongated, running in an approximate north-west to south-east direction with an average width of 8m and length of 40m. The land currently accommodates two single storey buildings serving as an electrical substation, with a gate fronting the site restricting access. An area of hard landscaping provides access from the street to the substation at the front with some soft landscaping and a tree. The rear is made up of soft landscaping with trees and hedges.
- 2.2 The site is situated just outside of Raynes Park Town Centre, with predominantly commercial properties to the west and residential to the north and east. No. 14 Pepys Road which adjoins to the south-west is a three storey office building whilst to the north-west No. 264 Worple Road is a two storey dwellinghouse with single storey garage that is built onto the substation.
- 2.3 The site is not located within a conservation area nor is it in anyway listed. The site is located within a controlled parking zone (Zone RPE) and has a public transport accessibility level (PTAL) of 5 (0 being the lowest and 6b being the best). The site is with in a very short walk of Raynes Park mainline rail station and a number of bus services. Cycleway C31 runs nearby and links to Kingston.

3. CURRENT PROPOSAL

- 3.1 The proposals form part of a series of four applications submitted for housing on the site, LBM Ref 21/P3428 is also for consideration by members at this meeting.
- 3.2 The proposal is for demolition of existing substation buildings and development of site to create 2 x 3 storey 3 bedroom dwellinghouses
- 3.3 The two houses would occupy the full width of the site with setback front elevations to reflect the bend in the road layout. Each house would have a small space to the front with facilities for refuse and secure cycle storage and a small area of greenery.

- 3.4 Front doors open into study/living room areas leading onto ground floor bathrooms and the main kitchen/living dining room areas to the rear, the rear element of which would be single storey, with doors leading out to rear patio areas. These would lead in turn to large rear gardens extending to the rear site boundary. The roofs of the single storey element would be part tiles and part glazed, sloping down from a central ridge line.
- 3.5 At first floor level there would be a modest double bedroom to the front and a large single bedroom to the rear. At second floor level each house would have the main large double bedroom to the front set under a gable fronted hipped roof with rear doors out to a rear terrace. This level would also accommodate a shower room. The roof forms to the rear would be flat and utilise green roof designs.
- 3.6 Proposed materials comprise a brick finish to most walls, zinc cladding to part of the rear extension at 2nd floor level. Powder coated aluminium windows and patio doors, powder coated aluminium/timber front doors, timber slatted doors to refuse and bicycle stores. For the roof the pitched roofs would use slate and the flat roofs rubber membrane and a green roof system
- 3.7 Proposed units

Unit	Type	Prop.GIA	Req. GIA	Prop amenity	Req Amenity
House 1 East	3B 5P on 3 floors	106m ²	99m ²	67m ²	50m ²
House 2 West	3B 5P on 3 floors	109m ²	99m ²	59m ²	50m ²

4. **PLANNING HISTORY**

- 4.1 21/P3428 - Demolition of existing electrical sub-station and the erection on site of 3 x self-contained flats with external amenity space and access to grounds of Langham Court – Reported to PAC
- 4.2 20/P2682 - Demolition of existing electrical sub-station and erection on site of new 3 bed dwellinghouse with front and rear gardens and access to grounds of Langham Court – Pending decision.
- 4.3 21/P2784 - Demolition of existing electrical sub-station and erection on site of new 4 bed dwellinghouse with front and rear gardens and access to grounds of Langham Court - Pending decision

5. CONSULTATION

Consultation letters sent to neighbours and site notice posted. Objections were received from 86 residents raising concerns relating to;

- Fire Safety; the site is a Fire and Access service access route to the rear of the site. Fire Engines and crews would use this access.
- Breach of lease agreements over pathways, gardens and accessways to and from the site.
- Unfair to residents to build over their communal gardens.
- The size will result in loss of light and amenity to a number (10) of flats in Langham Court
- By virtue of height and position would constitute a visually intrusive form of development
- Loss of privacy to lower level flats from the new house/s
- The new house/s will not benefit from good natural light
- Pressure on parking
- It is within the green corridor and provides important wildlife habitat
- Results in loss of tress and accessway
- Should retain the soft natural space
- No affordable housing
- It will increase risk of flooding
- Pedestrian access to Langham Court should be retained
- The proposed 3 bed houses will have little natural light from the rear, being enclosed by the buildings and trees either side and Langham Court to the back. Indoors and out will be very cramped due to the proposal to build two not one dwellings in the available space. This lack of amenity suggests a poor quality living environment for potential occupiers of the dwellings.
- Will impact light to rear garden of 284 Worple Road
- Loss of privacy from the rear roof terraces, they would need screening
- Various parts of the rear of 284 Worple Road are shared with the sub station
- Dividing boundary fence can't be higher than 1.18m to allow occupiers of 284 to drive safely from their garage
- The developer is just being greedy.
- Construction of the proposed houses would be a significant noise issue for the residents of Langham Court and adjacent properties.
- Arrival and departure of demolition vehicles, building materials, workers and staff will be a major issue, in terms of noise, parking and the safety of all other road users, on what is a blind bend at a complex junction (for both vehicles and pedestrians) on a major road.
- The proposed development necessitates the demolition of the existing two-part electrical substation, which supplies a great many residences and businesses in the area. The replacement of

this is likely to cause significant disruption to the power supplies for all those who are dependent upon it.

- New residents would have uncontrolled access to Langham Courts gardens and refuse facilities
- Devalue the flats and leaseholds

5.2 Flood Officer - No objections

5.3 Transport Planning - No objections

5.4 Tree Officer - Whilst there were no arboricultural objections seen to the scheme, the arboricultural reports should either be amended to incorporate the site visits, or attach conditions F5 & F8.

5.5 London Fire Brigade

The local fire service Station Officer undertook an initial site visit and commented that the access point is needed and would be used in the event of an incident taking place at the flats. The local Fire Station Officer Comments;

8/11/2021

“Sorry for the delay in responding. As discussed I visited the site after your visit. From the walk around that we did I would say that the access is needed and would be used in the event of an incident taking place at the flats. Although it would be possible at the moment to manoeuvre a fire appliance down the access route to the flats it would not be something that we would do due to the lack of hard standing and the close proximity of the road. It would be necessary however to retain some form of access so personnel with equipment could access the rear of the flats on foot. I should stress however that this is my opinion and if you need official documentation this can be arranged by our fire safety department. I have contacted them and am just waiting for their reply. I would assume they would take my assessment as the brigade stance unless there is some legislation that applies. I will forward their reply to you as soon as I have it”.

Officers sought further clarification as to whether the Fire Brigade Safety Department had any comments to make and the Station officer replied on

10/1/2022

I'm very sorry about the delay in getting back to you on this enquiry. I have managed at last to pin down our Fire Safety department. Unfortunately they can give no further insight into the application outside of what I said a couple of months back. This is because it is a planning application issue and as such they will

not comment on it. So from our perspective I can only reiterate what I stated before:

‘access is needed and would be used in the event of an incident taking place at the flats. Although it would be possible at the moment to manoeuvre a fire appliance down the access route to the flats it would not be something that we would do due to the lack of hard standing and the close proximity of the road. It would be necessary however to retain some form of access so personnel with equipment could access the rear of the flats on foot’.

Given the presence of other access points around Langham Court officers sought further clarification from the officer on this point and on

27/02/2022

“Sorry for the delay in coming back to you. In answer to your question I don’t think that this is a fire safety issue, as confirmed by our fire safety department. The reason for the opinion I gave was from a practical operational perspective. It could be better to have rear access to the flats for faster deployment of resources and the ability for a dual approach. However, if that wasn’t available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. This would then be the same for many other places where there is only front access.”

5.6 UK Power Networks

The company were consulted on the proposals as they would be involved in the removal of their electricity substations. It was confirmed that the stations are operational and whilst it would be physically possible to relocate them this would involve separate applications to them.

6. **POLICY CONTEXT**

Relevant policies in the London Plan 2021 are;

H1 (Increasing housing supply)

H2 (Small sites)

D1 (London’s form, character and capacity for growth)

D3 (Optimising site capacity through a design lead approach)

D5 (Inclusive design)

D6 (Housing Quality and standards)

D11 (Safety & Security)

D12 (Fire safety)

GG2 (Making the best use of land)

GG4 (Delivering Homes Londoners need)

GG6 (Increasing efficiency and resilience)

SI 2 (Minimising greenhouse gas emissions)

SI.3 (Sustainable drainage)
SI.5 (Water infrastructure)
S1.7 (Reducing waste and supporting the circular economy)
S1.8 (Waste capacity and net waste self-sufficiency)
T5 (Cycling)
T6.1 (Residential Parking),

Merton Local Development Framework Core Strategy (2011)

Relevant policies include:

CS 8 Housing choice.
CS 9 Housing targets
CS 11 Infrastructure
CS 13 Open space and nature conservation
CS 14 Design
CS 15 Climate Change
CS 17 Waste
CS 18 Transport
CS 20 Parking servicing and delivery

The relevant policies in the Council's Adopted Sites and Policies Plan 2014 are:

DM D1 Urban design and the public realm
DM D2 Design considerations
DM O2 Nature conservation, trees and hedges
DM T2 Transport impacts of development
DM T3 Car Parking and servicing standards

Other relevant documents/guidance

NPPF 2021

London Housing Supplementary Planning Guidance 2016.

7. PLANNING CONSIDERATIONS

7.1 The principal planning considerations in this case relate to the principle of residential development, fire safety, electricity supply infrastructure, design, the impact on neighbour amenity, standard of residential accommodation and trees.

7.2 Principle of development and the need for housing

7.2.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

7.2.2 The National Planning Policy Framework 2021, London Plan 2021 policy H1 (Increasing housing supply) and the Council's Core Strategy policy CS9 all seek to increase sustainable housing provision where it can be shown that an acceptable standard of accommodation will also provide

a mix of dwelling types. Policy D3 of the London Plan 2021 requires all development to make the best use of land by following a design-led approach that optimises the capacity of sites, including site allocations. Optimising site capacity means ensuring that development is of the most appropriate form and land use for the site.

7.2.3 The National Planning Policy Framework and London Plan policies H1 and H2 (Small sites) promote sustainable development that encourages the development of additional dwellings at locations with good public transport accessibility. This site is within ready walking distance of Raynes Park station and a number of bus routes. Core Strategy policies CS8 & CS9 seek to encourage proposals for well-designed and conveniently located new housing that will create socially mixed and sustainable neighbourhoods through physical regeneration and effective use of space. The principle of residential development is therefore considered acceptable.

7.2.4 Policy H1 of the London Plan 2021 has set Merton a ten-year housing target of 9,180 new homes. By providing 2 new family sized units the proposals would make a small but important contribution to meeting that target and providing much needed new housing. The proposal to introduce residential use to this site is considered to respond positively to London Plan and Core Strategy planning policies to increase housing supply, optimise sites and is therefore supported by Officers.

7.3 **Fire Safety**

7.3.1 London Plan 2021 policy D12 (Fire safety), is concerned with ensuring that new developments are not at risk from fire. It requires developments to provide suitable and convenient means of escape, and associated evacuation strategy for all, to identify suitably positioned unobstructed outside space and provide access for fire service personnel and equipment. Whilst it is acknowledged that the policy is aimed at primarily at ensuring fire safety at new developments it is considered relevant in ensuring that new developments do not impact on fire safety at other developments.

7.3.2 All the pending planning applications on the application site have all been accompanied by a high number of objections from the residents of Langham Court, which is a large block of flats to the rear of the site, concerned that the use of the site will hamper Fire Brigade access to them in the event of emergency.

7.3.4 As part of the application process, officers consulted the local London Fire Brigade Station Officer (full details in section 5.5 of this report) who, after initial reservations confirmed on the 27/02/2022 (following officer clarification in regard to the presence of other access points around Langham Court) that:

“Sorry for the delay in coming back to you. In answer to your question I don’t think that this is a fire safety issue, as confirmed by our fire safety department. The reason for the opinion I gave was from a practical operational perspective. It could be better to have rear access to the flats for faster deployment of resources and the ability for a dual approach. However, if that wasn’t available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. This would then be the same for many other places where there is only front access.”

- 7.3.5 As set out above, the London Fire Brigade raise no fundamental objection to the proposed scheme. They confirmed that they don’t think this is a fire safety issue, could be better to have rear access, however if that wasn’t available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. Therefore, based on the advice received, officers consider that there would be limited justification to refuse planning permission on fire safety grounds.
- 7.3.6 As a high number of local residents raised concerns with the very serious matter of fire safety, officers sought additional information and clarification from the applicant on this point. The applicant was therefore required to provide an independent fire strategy report with the application. Such reports are only usually required for major development however given the concerns raised by local residents, officers made a request for a fire strategy report in this instance. The applicants have submitted a Fire Strategy report compiled by R.Bosdet DMS, MIFireE (Member of the Institution of Fire Engineers) of Phoenix Executive Fire Advisory Associates that confirms that the existing access arrangements are adequate and that the proposed residential use of the site would not impact fire safety for the residents of Langham Court.
- 7.3.7 If planning permission is granted by members of the planning committee, it should be noted that the site of the flats at Langham Court can still be accessed along both Langham Road and Wyke Road by vehicles and pedestrians.
- 7.3.8 In conclusion, officers have proactively sought the views of professionals in terms of fire risk. Both the London Fire Brigade and the applicants independent fire expert have confirmed no fundamental objections or concerns with fire safety. Therefore, based on the information before officers, officers are content that the proposals would not result in an unacceptable increase in risk to the safety of Langham Court Residents in the event of fire to justify a refusal of planning permission. It should also be noted that the new buildings would be subject of separate Building Regulations approvals process which addresses fire safety matters.

7.4 **Electricity supply infrastructure**

- 7.4.1 Core strategy policy CS 11 (Infrastructure) states out that the Council will work with utility suppliers to ensure the safe and secure transportation of utilities such as electricity is not compromised.
- 7.4.2 UK Power Networks have confirmed that the substation is an active facility but have raised no objection to the application. UK Power Networks have not confirmed if the removal of the substation would result in electricity being compromised, but it is assumed that alternative facilities would need to be provided before this site was deactivated. The applicant has indicated that alternative provision could be provided within the site near to the refuse facilities associated with Langham Court in the further southwestern corner of the Langham Court land. However, this is only indicative and has not been agreed with UK Power Networks as being a feasible option.
- 7.4.3 In order to ensure that there are no negative impacts on local electricity supply it is recommended that a condition be attached that requires confirmation that the applicant has got the necessary approvals from UK Power Networks and that no development shall be undertaken until alternative electrical substation facilities have been provided and are operational.

7.5 **Design**

- 7.5.1 London Plan 2021 policy D3 (Optimising site capacity through a design lead approach), Core Strategy policy CS14 (Design) and SPP policy DM D2 (Design considerations) require developments to relate positively and appropriately to the siting, scale, proportions and massing of surrounding buildings and existing street patterns and which would enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions.
- 7.5.2 The site has an industrial character from the functional substation structures and is located on a bend in the road between a terrace of houses along Worple Road and a more modern office block on Pepys Road. This transitional nature of the site is considered to allow for a greater diversity of design options than would be the case otherwise. The proposed design is however considered attractive and whilst narrower than other houses along this part of the road they are considered to reflect the height and front gable feature of the front elevations of the adjacent houses and the materials of the offices.
- 7.5.3 Conditions requiring details of materials to be approved and restricting the provision of cables, wires, aerials, pipe work on the front elevation are recommended to ensure a satisfactory appearance for the development and to guard against value engineering. Subject to those

factors the appearance and character of the houses are considered acceptable.

7.6 The impact on neighbour amenity

7.6.1 SPP policy DM D2 and London Plan policy D3 require that proposals do not have a negative impact on neighbour amenity from loss of light, privacy, visual intrusion or increased disturbance and that people feel comfortable with their surroundings.

7.6.2 In order to safeguard neighbour amenity and to restrict over development of the site conditions removing permitted development rights for new windows and extensions to the houses are recommended.

Langham Court

7.6.3 The occupiers of a number of the flats in Langham Court objected that they would suffer from a loss of light although those flats are at least a minimum distance of 23.9m from the rear wall of the closest house. Given this distance and the orientation of the site to those flats it is considered that little weight can be afforded those objections or the proposed buildings being visually intrusive.

7.6.4 In terms of overlooking and impact on privacy for the residents of Langham Court, the closest 1st floor window is approx. 31m away from the closest part of Langham Court and the closest 2nd floor window is at least 33m away. The level of separation would ensure that there would be no undue overlooking or loss of privacy.

284 Worples Road

7.6.5 The design of the proposed pair of semi-detached houses would have a staggered front and rear building line. The proposed house closest to this neighbour would have its upper floor levels align with the rear elevation of the neighbours attached garage. At ground floor level, the proposed house does project 5.9m beyond the rear of this neighbour, however the ground floor level would only have a parapet flank wall height of 2.1m (only 0.1m higher than a permitted development boundary fence). In addition, the pitched roof form thereafter is modest in height and slopes up from the boundary to ensure that the ground floor projection would not appear visually intrusive or overbearing.

7.6.6 The more westerly of the two houses on the application site does project 3.5m beyond the rear building line of the other new house, however the flank wall would be inset 3m from the boundary thereby offering a good level of separation to help preserve light levels and ensure there would be no visual harm. The proposed houses have the potential to impact on light given it is to the south of that neighbour, however the existing office block (14 Pepys Road) is higher and deeper than the proposal and would

already cast a degree of shadowing over that garden. Given the above it is considered that there would be no undue loss of light.

- 7.6.7 Objectors raised concerns of overlooking and loss of privacy from the houses and in particular the two upper floor amenity areas. In order to ensure there is no undue loss of privacy and overlooking, these upper floor terraces would need to be fitted with appropriate screening, details can be secured via condition.

14 Pepys Road

- 7.6.8 Commercial property formed of offices and therefore no undue loss of amenity.

Conclusion (amenity)

- 7.6.9 In view of the above factors the proposals are not considered to be materially harmful to the amenity of neighbouring occupiers and are therefore considered to be acceptable in this regard

7.7 Standard of Residential Accommodation

- 7.7.1 SPP Policy DM D2, Core Strategy 2011 policies CS 9 Housing Provision and CS 14 Design and London Plan policy D6 Housing standards are all policies that seek to provide additional good quality residential accommodation with safe and secure access that meets minimum internal and external space standards.

- 7.7.2 Accommodation schedule

Unit	Type	Prop.GIA	Req. GIA	Prop amenity	Req Amenity
House 1 East	3B 5P on 3 floors	106m ²	99m ²	67m ²	50m ²
House 2 West	3B 5P on 3 floors	109m ²	99m ²	59m ²	50m ²

- 7.7.3 Both houses are dual aspect and offer a well set out range of rooms with good levels of natural light and they exceed both the relevant internal GIA and exterior amenity space standards for properties of this size. The proposed accommodation is consequently considered acceptable.

7.8 Parking, servicing and deliveries

- 7.8.1 Planning Policy T1 (Strategic approach to transport) of the London Plan 2021 states that the delivery of the Mayor's strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041. All development should make the most effective use of land, reflecting its connectivity and accessibility by existing and future public

transport, walking and cycling routes, and ensure that any impacts on London's transport networks and supporting infrastructure are mitigated.

- 7.8.2 Planning Policy DM T2 (Transport impacts of development) of Merton's Sites and Policies Plans seeks to ensure that development is sustainable and has minimal impact on the existing transport infrastructure and local environment.

Cycle parking

- 7.8.3 Planning Policy T5 (Cycling) of the London Plan 2021 states that development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle. Developments should provide cycle parking at least in accordance with the minimum standards set out in Table 10.2. In accordance with Table 10.2, residential dwellings should provide 1 space per studio/1 person 1 bedroom dwelling, 1.5 spaces per 2 person 1 bedroom dwelling and 2 spaces per all other dwellings.

- 7.8.4 Each house would have their own dedicated double space cycle storage in their front garden. The applicant has provided sufficient cycle spaces and facilities; therefore the proposal complies with policy T5 of the London Plan 2021.

Car parking

- 7.8.5 Planning Policy T6 (Car parking) of the London Plan 2021 states that Carfree development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Car-free development has no general parking but should still provide disabled persons parking in line with standards set out in policy T6.1 (Residential parking). Planning Policy DM T3 (Car parking and servicing standards) of Merton's Sites and Policies Plan also promotes car free development in areas which benefit from good access to public transport (generally PTAL 4 or above) and be within an existing controlled Parking Zone.

- 7.8.6 The application site has a PTAL rating of 5 (good) and within a Controlled Parking Zone RPE. Therefore, the development is considered to be suitable as a permit free development as this would take pressure off existing car parking zones and promote sustainable modes of transport. The applicant has agreed to the permit free requirement, this can be controlled via a S106 agreement restricting permits.

7.9 Bin and Recycling Storage

- 7.9.1 Planning Policy D6 (Housing quality and standards) of the London Plan 2021 states that housing should be designed with adequate and easily accessible storage space that supports the separate collection of dry

recyclables (for at least card, paper, mixed plastics, metals, glass) and food waste as well as residual waste.

- 7.9.2 Refuse bins are proposed to the front gardens of each house, which is considered a suitable location. This would be convenient on collection days when needing to wheel/bring forward to the kerbside. The proposal indicates the inclusion of a residual waste bin; paper and card bin; dry recycling container; and a food waste bin. Therefore the proposal complies with policy CS17 of the Council's Core Strategy Policy and policy SI 7 and SI 8 of the London Plan 2021 although a condition requiring details of the design to be approved is recommended to ensure suitability of appearance and functionality.

8. SUSTAINABLE DESIGN AND CONSTRUCTION

- 8.1 Merton's Core Planning Strategy (2011) Policy CS15 outlines how all minor and major development, including major refurbishment, should demonstrate: how the proposal makes effective use of resources and materials, minimises water use and CO2 emissions; makes the fullest contribution to minimising carbon dioxide emissions in accordance with the energy hierarchy and designed to withstand the long term impacts of climate change.
- 8.2 London Plan Policies SI 2, SI 5 and Merton's Sustainable Design and Construction Explanatory note, expects developments to achieve carbon reductions beyond Part L from energy efficiency measures alone to reduce energy demand as far as possible.
- 8.3 For minor residential developments, development is required to achieve a 19% improvement on Part L of the Building Regulations 2013 and water consumption should not exceed 105 litres/person/day.
- 8.4 The application was submitted with a sustainable design and construction statement outlining how the development has been designed in order to meet these requirements for energy and water savings. It is recommended that a condition be attached requiring details be submitted to demonstrate that a sustainable building has been provided in accordance with relevant policy.

9. Trees

- 9.1 The applications were accompanied by Arboricultural Impact Assessments (AIA) which confirmed that the proposals would involve the removal of three trees (A Cat B Sycamore, a Cat C Walnut and Cat C Elder) and a shrub (Privet hedge). Some basic tree protection measures and working methodology (in accordance with BS 5837:2012) will ensure the retained and third-party trees and hedges are not detrimentally affected during construction. The Council's Tree Officer has confirmed no objection to the proposal subject to conditions.

9.2 Whilst landscaping details would be secured by condition the AIA has suggested the planting of two Silver Birch to the front of the site along with landscaping. The new trees will be of standard size (about 2-3m high), planted in full accordance with current British Standards (BS 8545: From Nursery to Independence in the Landscape) and once planted, the trees will be regularly maintained (watered and weeded during the spring and summer months) for at least 5 years or until established. On balance, whilst there would be the removal of three trees, the grade of the trees is low and offers limited amenity value in this instance. In that assessment is the fact that new tree planting can be secured via planning condition.

10. Other matters

10.1 The existing site is partially open to wildlife but it is not designated as any form of 'green' amenity facility and therefore its loss would not constitute grounds for refusal.

10.2 The proposals would impact on the garage of the adjoining neighbour and therefore this would be a matter that would require a detailed Party Wall agreement to be entered into between the applicant or any future owner of the site and the owner of 284 Worples Road setting out a methodology for the demolition and rebuild of the party wall.

10.3 The area is at low risk of flooding and therefore development could not be refused on the principle of a development at this location.

10.4 Building on land within the demise of Langham Court would be a civil matter between the leaseholders and freeholder and the impact on the value of nearby properties is similarly not a planning matter.

10.5 The proposal is for two houses and therefore does not fall subject to the threshold for being subject to affordable housing contributions/provision.

11. CONCLUSION

11.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

11.2 The development of the site would provide new housing for which there is a recognised need. The two houses are considered to be well and attractively designed and exceed the minimum requirements for internal and external space. The bulk, scale, indicative materials, massing and siting of the houses is considered to be of an acceptable appearance and not to have a negative impact on neighbour amenity.

- 11.3 There are considered to be no justifiable reasons to refuse the application on the grounds of fire safety and subject to the provision of an alternative electricity substation prior to the demolition of the existing buildings the proposals will not negatively impact utility supplies.
- 11.4 The site is ideally located in Raynes Park town centre with its local facilities and very good public transport connections and therefore is suitable for being a permit free development in order to reduce reliance on the car and to increase pressure on parking locally.
- 11.5 The proposal are therefore considered to be in accordance with Adopted Sites and Policies Plan, Core Planning Strategy and London Plan policies. The proposal is therefore recommended for approval subject to conditions and S106 agreement.

RECOMMENDATION

GRANT PLANNING PERMISSION subject to –

1. Subject to conditions and the completion of a Section 106 Agreement covering the following heads of terms:-

1. Permit Free.

2. The developer agreeing to meet the Councils costs of preparing, drafting and monitoring the Section 106 Obligations.

And the following conditions:

1. Commencement of Development
2. Approved Plans; Site location plan and drawings, 1141P- 20, 40, 200, 201, 210, 211, 212 & 213
3. No development shall commence until alternative electrical substation facilities, as confirmed by UK Power Networks, to serve the local area have been provided and made fully operational before any works have commence on site.

Reason, to ensure the continuous provision of electricity utilities in accordance with policy CS11 of the Merton Core Strategy 2011.

4. External Materials to be Approved – No development, other than demolition, shall take place until details of particulars and samples of the materials to be used on all external faces of the development hereby permitted, including window frames and doors (notwithstanding any materials specified in the application form and/or the approved drawings), have been submitted to the Local Planning Authority for approval. No works which are the subject of this condition shall be carried out until the details are

approved, and the development shall be carried out in full accordance with the approved details.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for Merton: policies D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

5. C07 Refuse & Recycling – No development shall be occupied until details a refuse management plan for the management and collection of waste on site have been submitted in writing for approval to the Local Planning Authority.

Reason: To ensure the provision of satisfactory management methods for the storage and collection of refuse and recycling material and to comply with the following Development Plan policies for Merton: policies SI 7 and D6 of the London Plan 2021, policy CS17 of Merton's Core Planning Strategy 2011 and policy DM D2 of Merton's Sites and Policies Plan 2014.

6. External Lighting – Any external lighting shall be positioned and angled to prevent any light spillage or glare beyond the site boundary.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policies DM D2 and DM EP4 of Merton's Sites and Polices Plan 2014.

7. Construction Times – No demolition or construction work or ancillary activities such as deliveries shall take place before 8am or after 6pm Mondays - Fridays inclusive, before 8am or after 1pm on Saturdays or at any time on Sundays or Bank Holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policy D14 of the London Plan 2021 and policy DM EP2 of Merton's Sites and Polices Plan 2014.

8. (Demolition and Construction Method Statement) – No development shall take place until a Demolition and Construction Method Statement has been submitted to, and approved in writing by, the Local planning authority. The approved Statement shall be adhered to throughout the demolition and construction period.

The Statement shall provide for:

- hours of operation
- the parking of vehicles of site operatives and visitors

- loading and unloading of plant and materials
- storage of plant and materials used in constructing the development
- the erection and maintenance of security hoarding including decorative -displays and facilities for public viewing, where appropriate
- wheel washing facilities
- measures to control the emission of noise and vibration during construction.
- measures to control the emission of dust and dirt during construction/demolition
- details of the demolition and rebuilding of the party wall
- a scheme for recycling/disposing of waste resulting from demolition and construction works

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

9. Construction Logistics Plan – Prior to the commencement of the development hereby permitted, a Demolition/Construction Logistics Plan (including a Construction Management plan in accordance with TfL guidance) shall be submitted to and approved in writing by the Local Planning Authority. The approved Statement shall be adhered to throughout the construction period unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

10. F1 No development shall take place until full details of a landscaping and planting scheme has been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved before the commencement of the use or the occupation of any building hereby approved, unless otherwise agreed in writing by the Local Planning Authority. The details shall include on a plan, full details of the size, species, spacing, quantities and location of proposed plants, together with any hard surfacing, means of enclosure, and indications of all existing trees, hedges and any other features to be retained, and measures for their protection during the course of development.

11. F5 No development [including demolition] pursuant to this consent shall commence until an Arboricultural Method Statement and Tree Protection Plan, drafted in accordance with the recommendations and guidance set out in BS 5837:2012 has been submitted to and approved in writing by the Local Planning Authority and the approved details have been installed. The details and measures as approved shall be retained and maintained, until the completion of all site operations.
12. F8 Site supervision: The details of the Arboricultural Method Statement and Tree Protection Plan shall include the retention of an arboricultural expert to supervise, monitor and report to the LPA not less than monthly the status of all tree works and tree protection measures throughout the course of the construction period. At the conclusion of the construction period the arboricultural expert shall submit to the LPA a satisfactory completion statement to demonstrate compliance with the approved protection measures.
13. B5 No development shall take place until details of all boundary walls or fences are submitted in writing for approval to the Local Planning Authority. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details are approved and works to which this condition relates have been carried out in accordance with the approved details. The walls and fencing shall be permanently retained thereafter.
14. B4 No development shall take place until details of the surfacing of all those parts of the site not covered by buildings or soft landscaping, including any parking, service areas or roads, footpaths, hard and soft have been submitted in writing for approval by the Local Planning Authority. No works that are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details have been approved and works to which this condition relates have been carried out in accordance with the approved details.
15. H07 Cycle Parking – No development shall not be occupied until the secure cycle parking facilities for the occupants of, and visitors to, the development shown on the approved drawings have been fully implemented and made available for use prior to the first occupation of the development and thereafter retained for use at all times.

Reason: To ensure satisfactory facilities for cycle parking are provided and to comply with the following Development Plan policies for Merton: policy T5 of the London Plan 2021, policy CS18 of Merton's Core Planning Strategy 2011 and policy DM T1 of Merton's Sites and Policies Plan 2014.

16. The residential units hereby approved shall not be occupied until it has demonstrated to and approved in writing by the Local Planning Authority that they have achieved an energy efficiency standard of a minimum of 19% CO2 improvement over Building Regulations requirements Part L 2013 (TER Baseline) and, as a minimum, a water efficiency standard of not more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply with the following Development Plan policies for Merton: policy SI 2 of the London Plan 2021 and policy CS15 of Merton's Core Planning Strategy 2011.

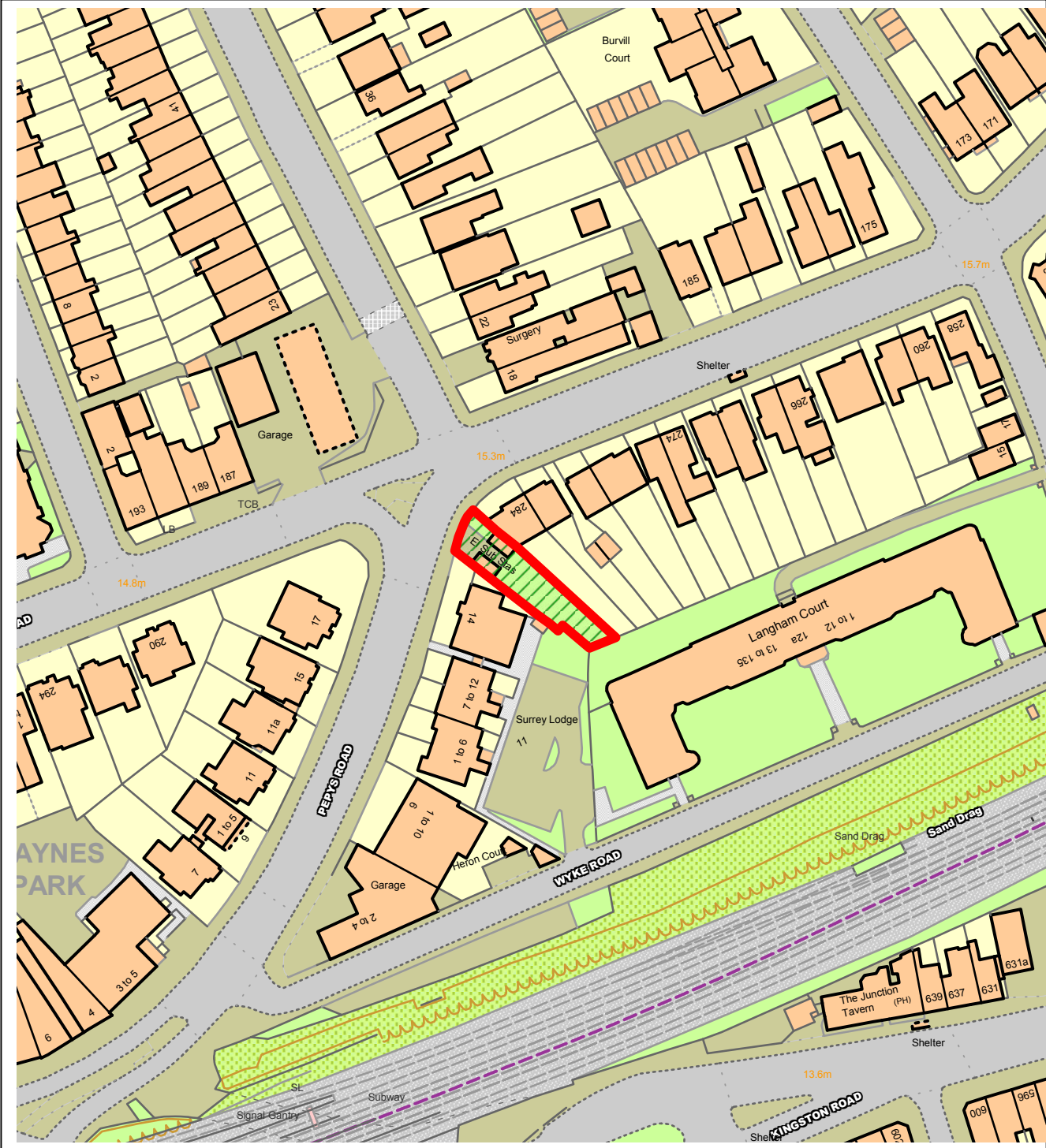
17. C1 Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no extension, enlargement or other alteration of the dwellinghouse other than that expressly authorised by this permission shall be carried out without planning permission first obtained from the Local Planning Authority.
18. C2 Notwithstanding the provisions of the Town and Country Planning (General Permitted Development)(England) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no window, door or other opening other than those expressly authorised by this permission shall be constructed in any easterly or southerly facing elevations without planning permission first being obtained from the Local Planning Authority.
19. C5 No cables, wires, aerials, pipe work (except any rainwater down pipes as may be shown on the approved drawings) meter boxes or flues shall be fixed to any elevation facing a highway.
20. C9 The screening or enclosure to the balconies as shown on the approved plans shall be implemented before the development is first occupied and retained permanently thereafter.

21. B6 No development shall take place until details of the proposed finished floor levels of the development, together with existing and proposed site levels, have been submitted to and approved in writing by the Local Planning Authority, and no development shall be carried out except in strict accordance with the approved levels and details.
22. F9 The hardstanding hereby permitted shall be made of porous materials, or provision made to direct surface water run-off to a permeable or porous area or surface within the application site before the development hereby permitted is first occupied or brought into use.
23. C6 No development shall take place until a scheme for the storage of refuse and recycling has been submitted in writing for approval to the Local Planning Authority. No works which are the subject of this condition shall be carried out until the scheme has been approved, and the development shall not be occupied until the scheme has been approved and has been carried out in full. Those facilities and measures shall thereafter be retained for use at all times from the date of first occupation.
24. No development (other than demolition) approved by this permission shall be commenced until a scheme for the provision of surface and foul water drainage has been implemented in accordance with details that have been submitted to and approved in writing by the LPA. Before these details are submitted an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system (SuDS) to ground, watercourse or sewer in accordance with drainage hierarchy contained within the London Plan Policy SI.3 and the advice contained within the National SuDS Standards. Where a sustainable drainage scheme is to be provided, the submitted details shall:
- i. provide information about the design storm period and intensity, the method employed to delay (attenuation provision of no less than 13m³) and control the rate of surface water discharged from the site to greenfield runoff rates (no more than 5l/s), and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - ii. include a timetable for its implementation; and
 - iii. provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption authority and any other arrangements.

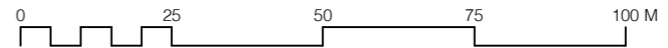
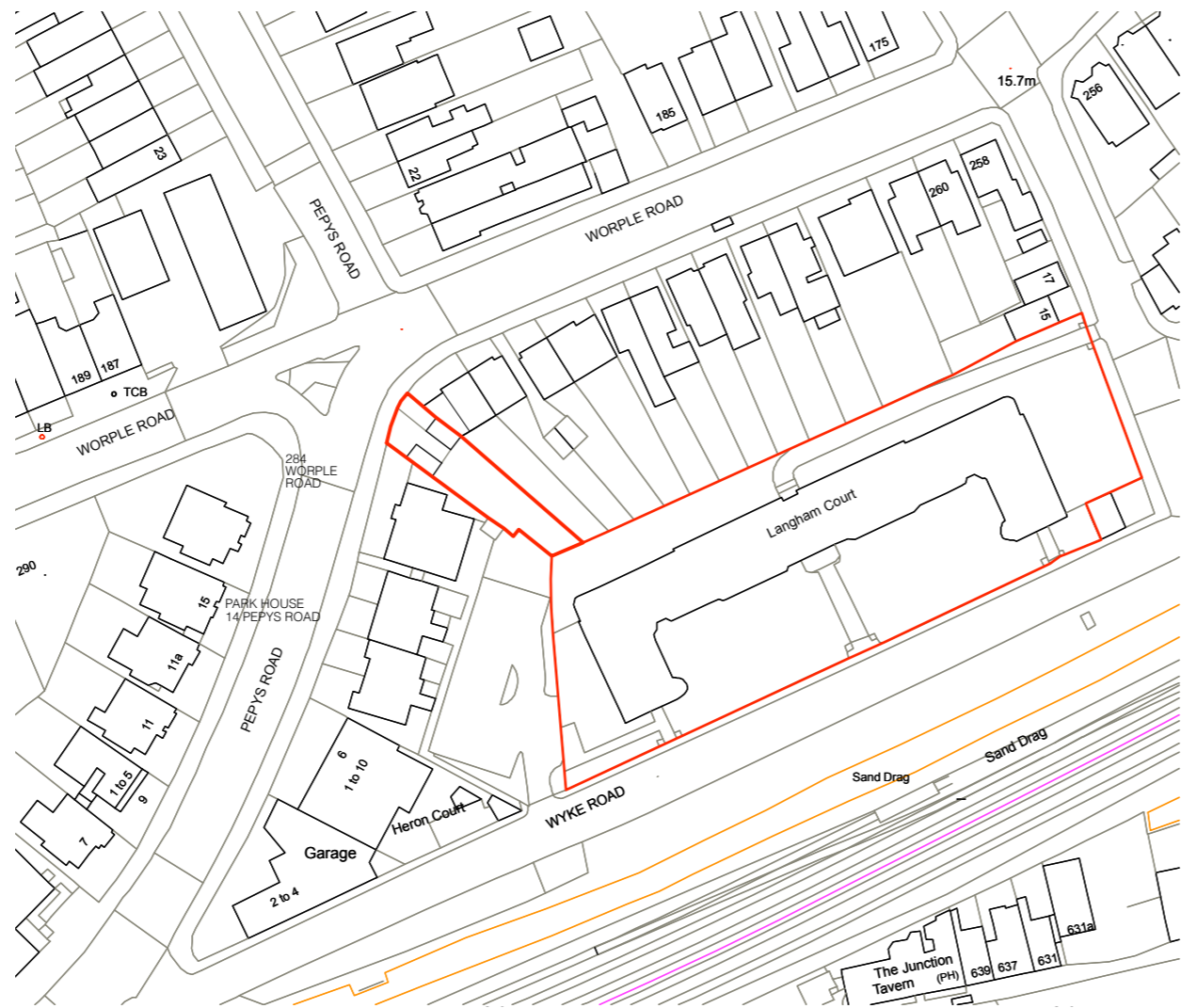
Reason: To reduce the risk of surface and foul water flooding and to ensure the scheme is in accordance with the drainage hierarchy of London Plan policy SI.3 and the

National SuDS standards and in accordance with policies CS16 of the Core Strategy and DMF2 of the Sites and Policies Plan.

NORTHGATE SE GIS Print Template

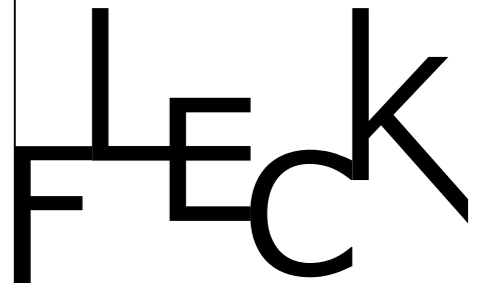


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CLIENT: _____

A Azouz

PROJECT: _____

Town houses, Pepys Road SW20

STATUS: _____

PLANNING

DRAWING: _____

Site location plan

DRAWN BY: _____ DATE: _____

C F

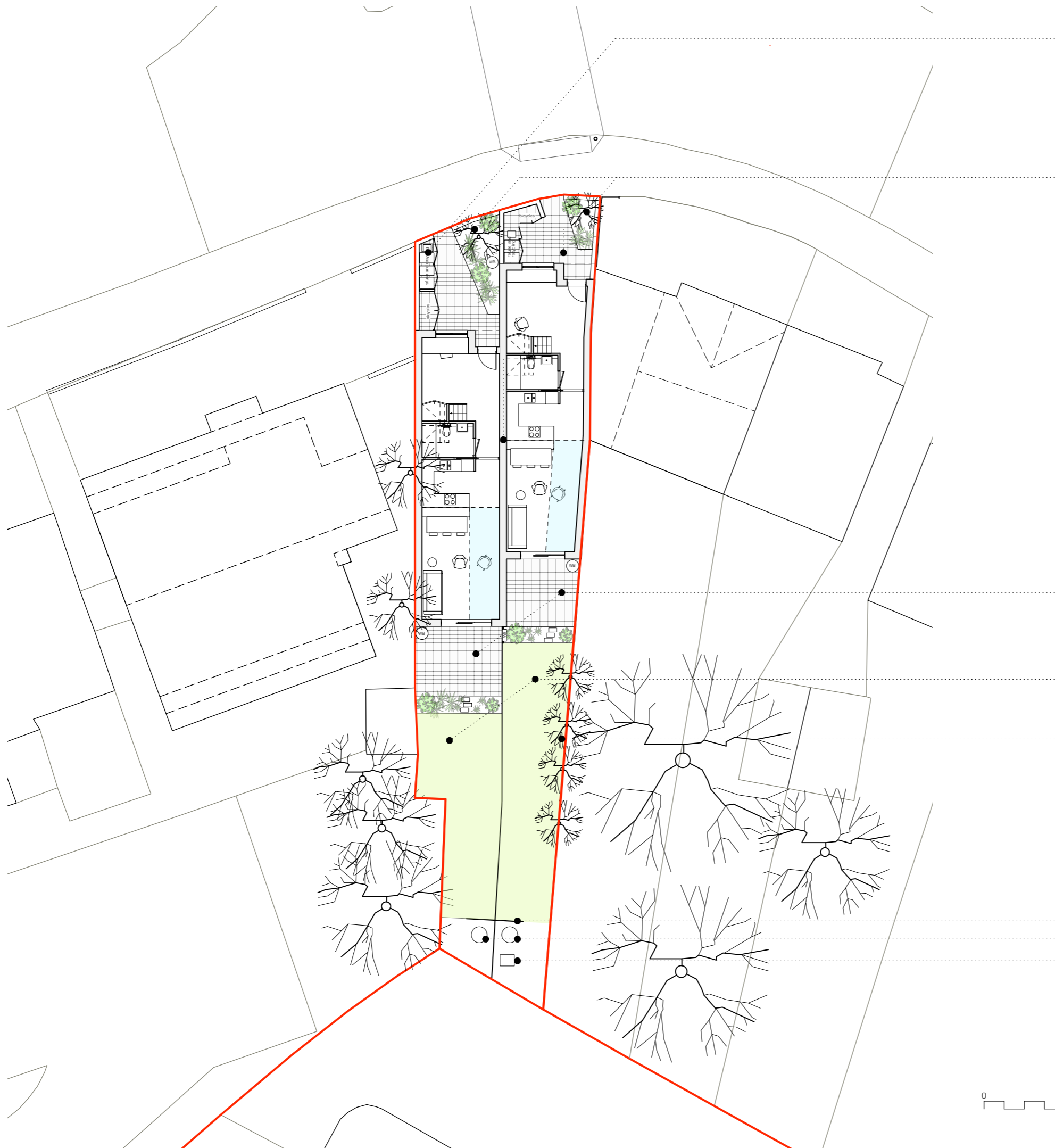
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refuse store with green roof

bicycle store with green roof and electric charging point

paving with permeable joins laid to fall toward planting area

planting areas containing native planting helping to slow and reduce surface water run off and improve biodiversity of site

garage belonging to 284 Worple Road

semi detached house at 284 Worple Road

paving with permeable joins laid to fall toward planting areas

lawn with perimeter planting against boundaries.

existing planting along boundary with 284 Worple Road to be retained and enhanced

willow screen

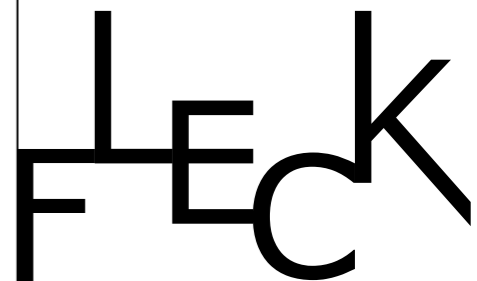
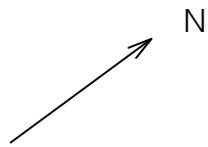
composting bins

garden waste bin

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WB rainwater harvesting (water butt)



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STATUS:

PLANNING

DRAWING:

Proposed block plan

DRAWN BY: DATE:

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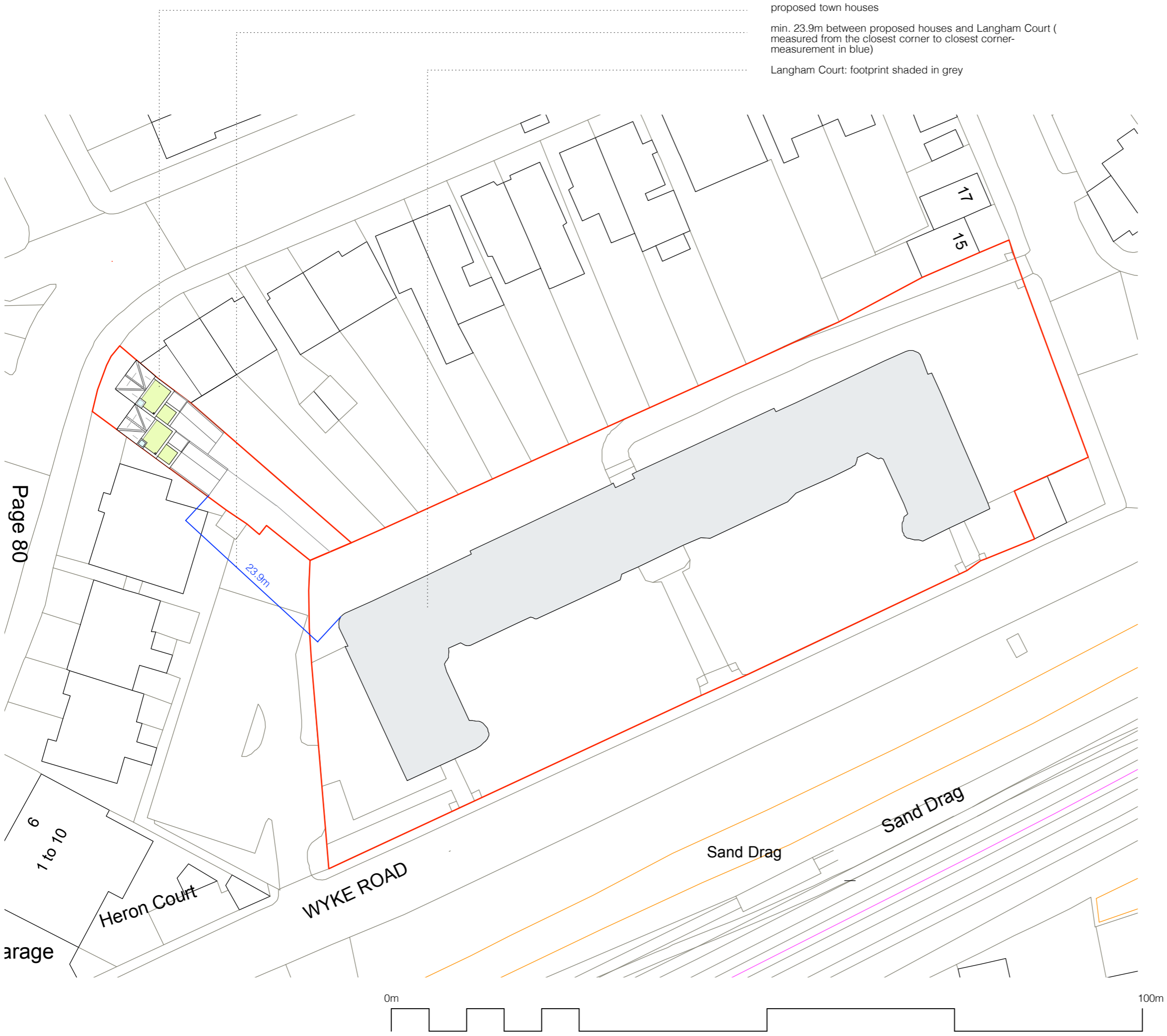
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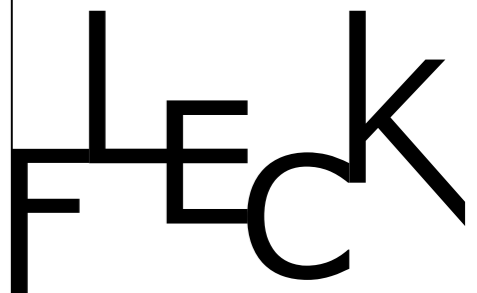




proposed town houses
 min. 23.9m between proposed houses and Langham Court (measured from the closest corner to closest corner - measurement in blue)
 Langham Court: footprint shaded in grey

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CLIENT:
 A Azouz
 PROJECT:
 Town houses, Pepys Road SW20

STATUS:
 PLANNING

DRAWING:
 Wider block plan

DRAWN BY: C F DATE: 0 6 0 4 2 1

SCALE: 1 : 5 0 0 @ A 3

DRAWING NUMBER: 1 1 4 1 P REV: 3 0

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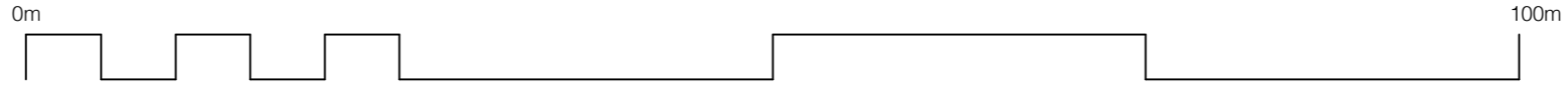
garage

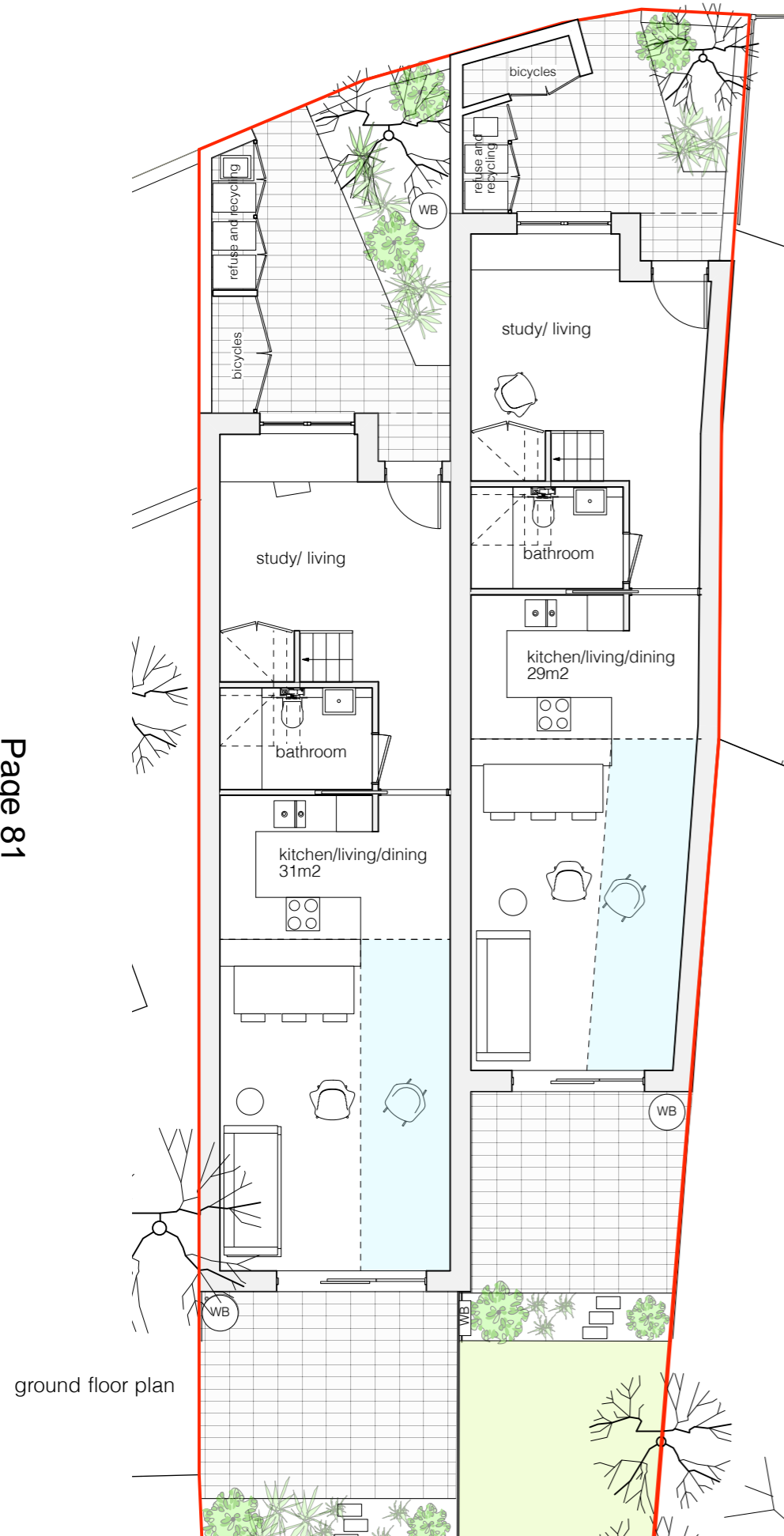
Heron Court

WYKE ROAD

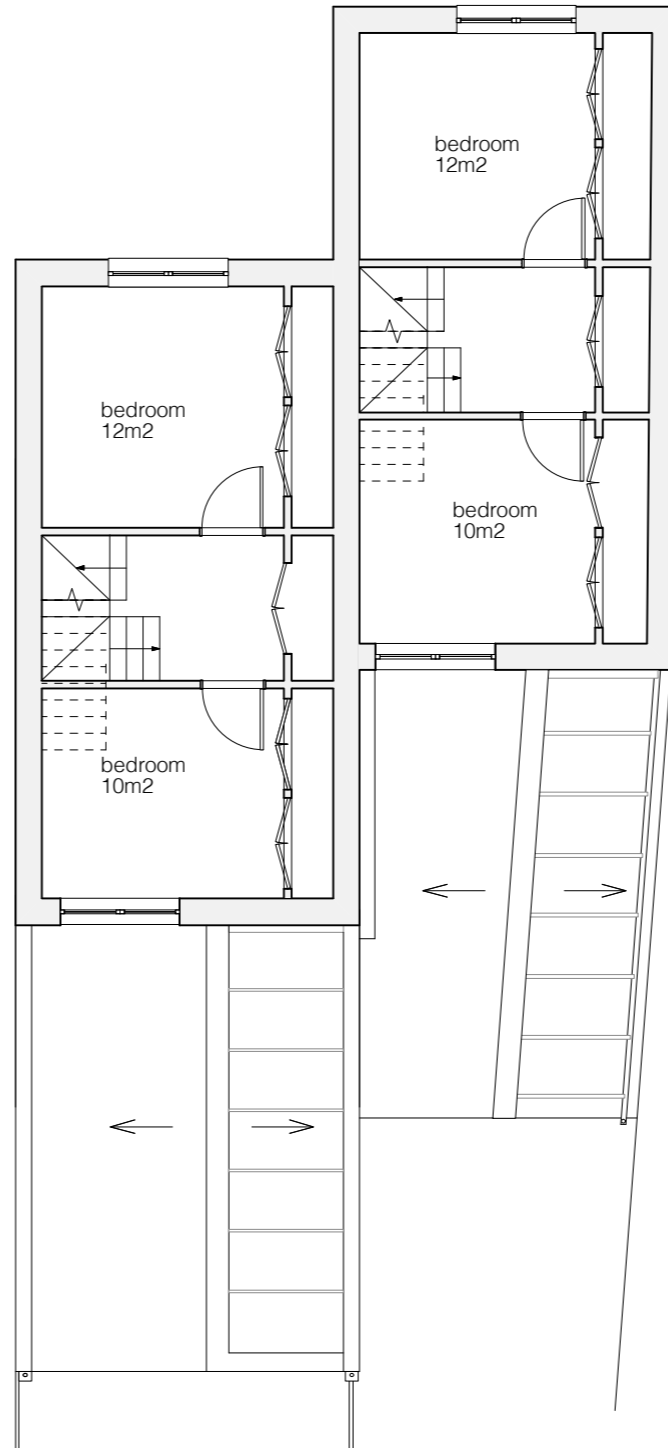
Sand Drag

Sand Drag

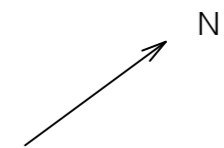




ground floor plan

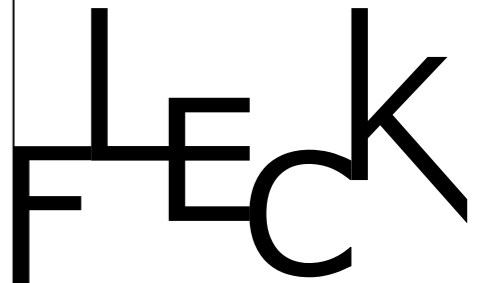


first floor plan



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Town houses, Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed plans
Ground and 1st floor

DRAWN BY: DATE:

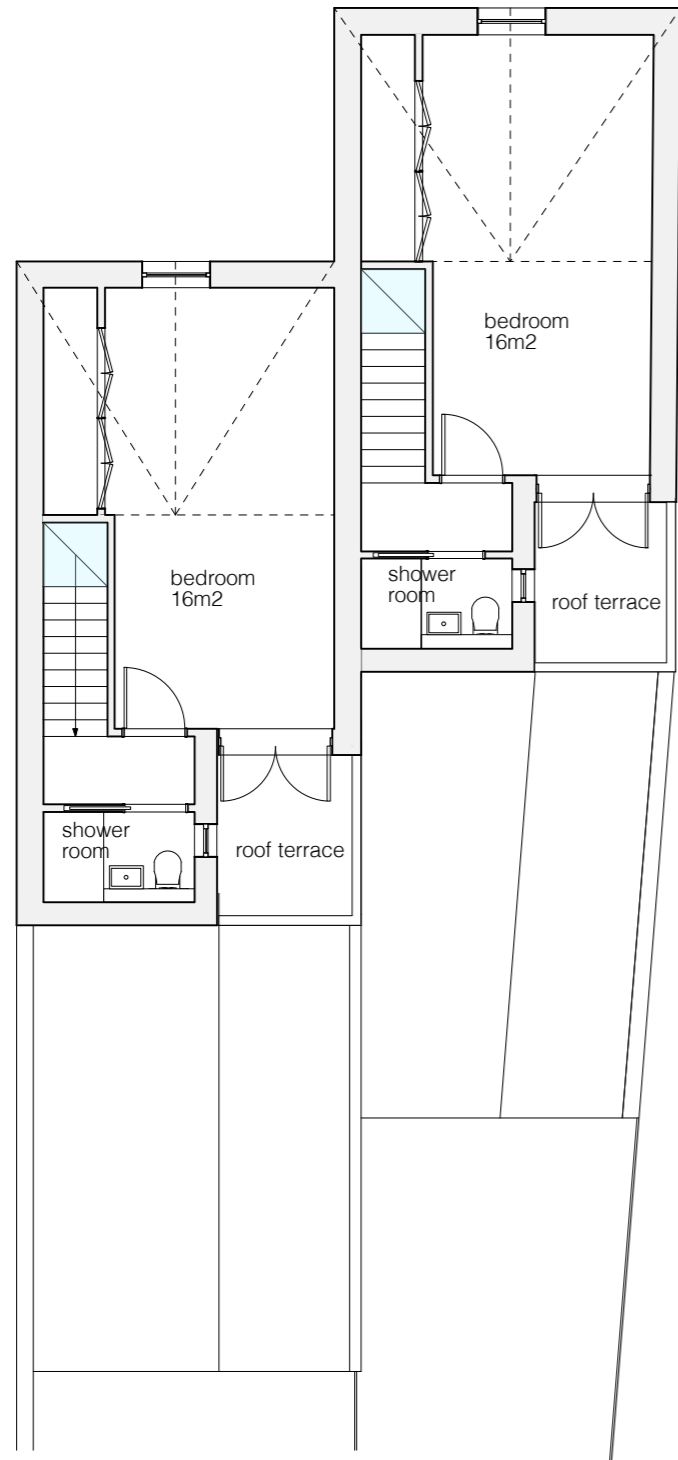
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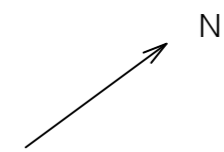
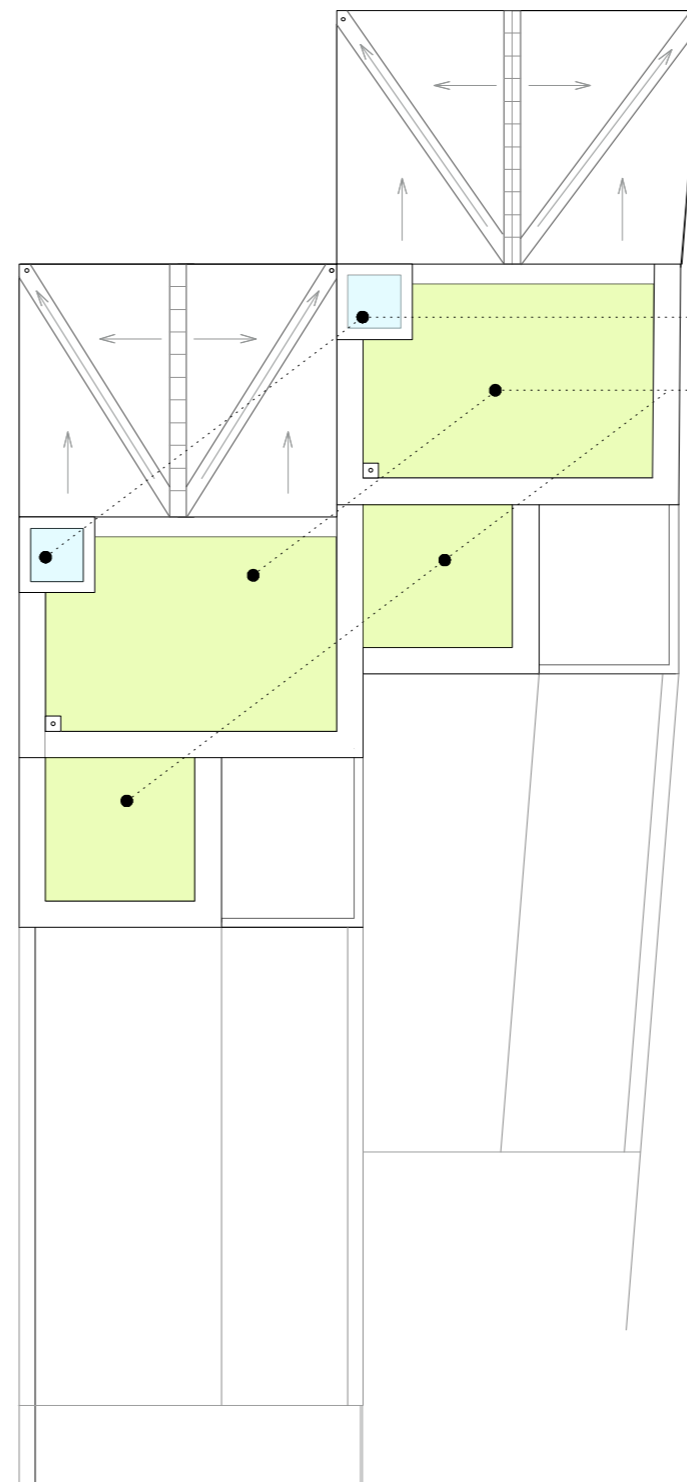
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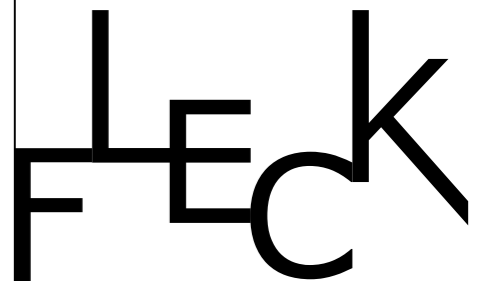


roof plan



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Town houses, Pepys Road SW20

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PLANNING

DRAWING: _____

Proposed plans
 2nd floor and roof

DRAWN BY: DATE:

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PROJECT:
 Town houses, Pepys Road SW20

STATUS:
 PLANNING

DRAWING:
 Proposed front elevation

DRAWN BY: DATE:
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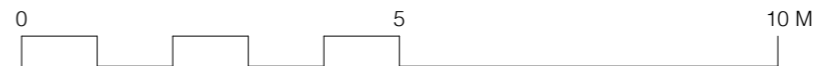


284 Worple Road

Substation site

14 Pepys Road





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Town houses, Pepys Road SW20

STATUS: _____

PLANNING

DRAWING: _____

Proposed rear elevation

DRAWN BY: _____ DATE: _____

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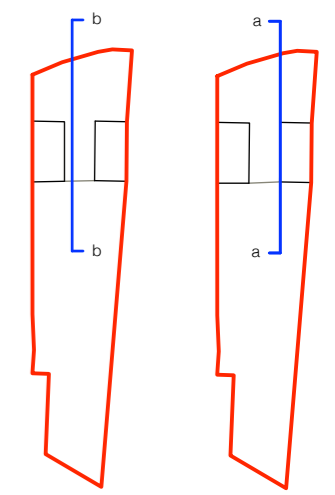
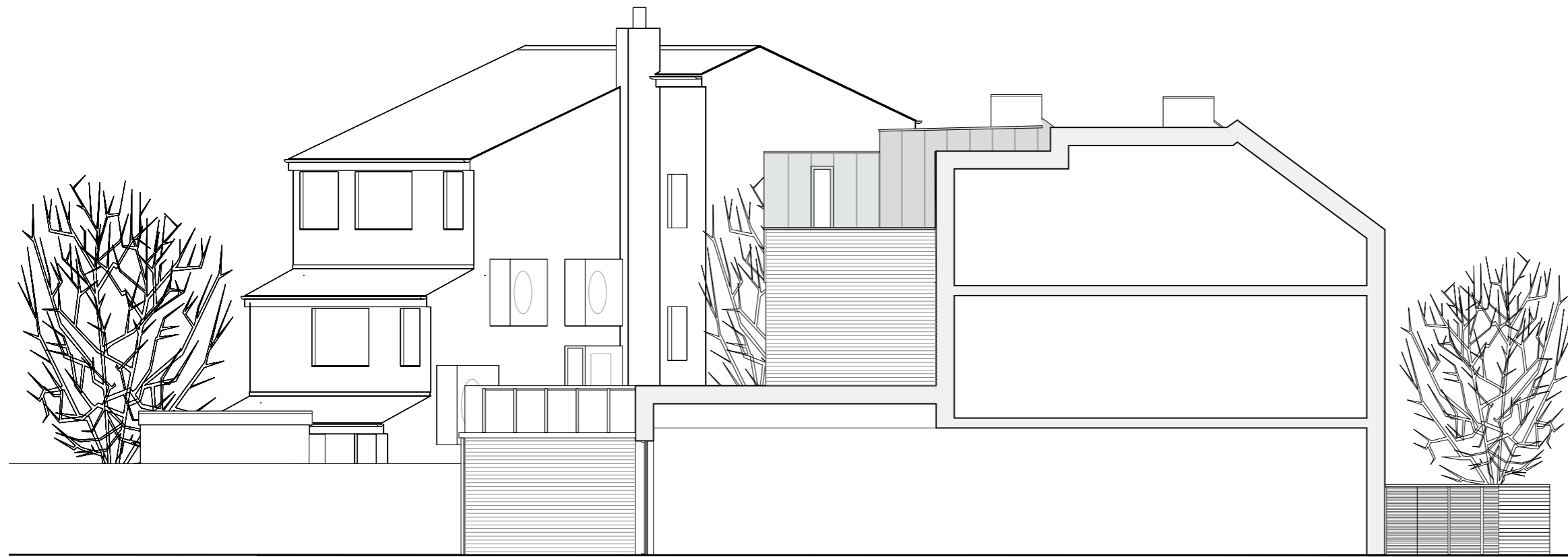
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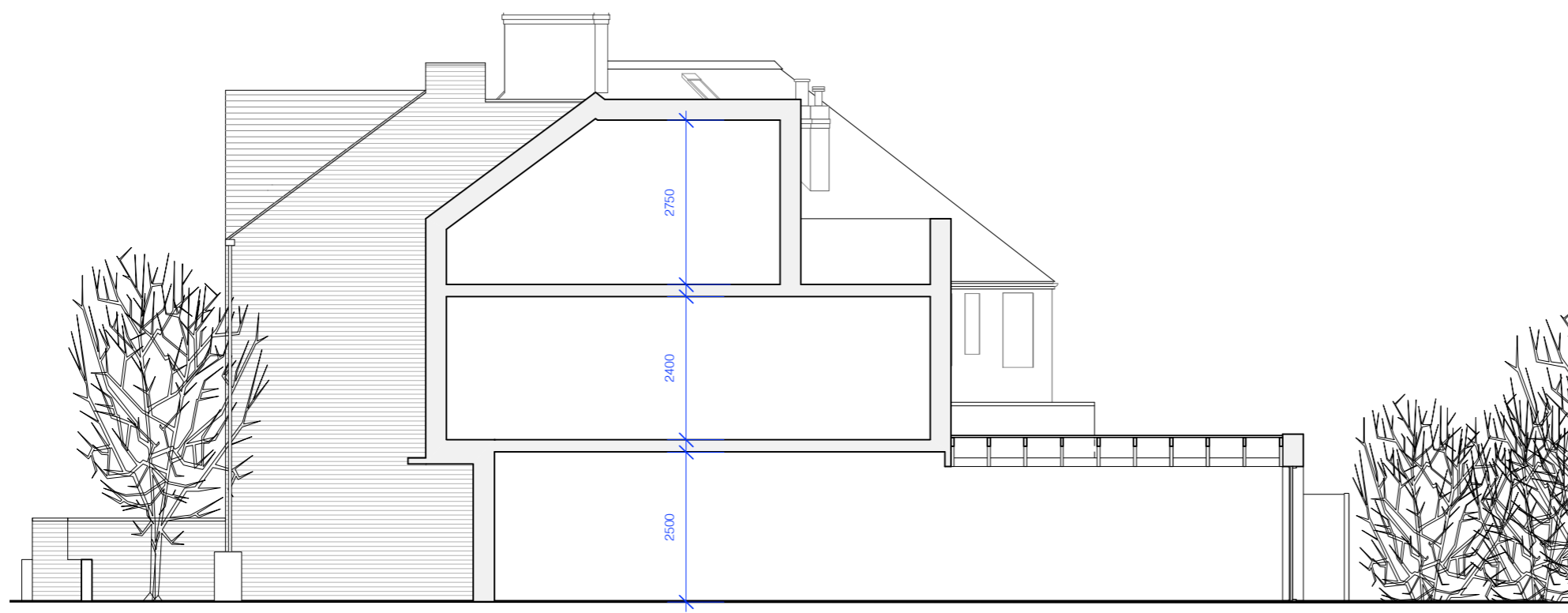
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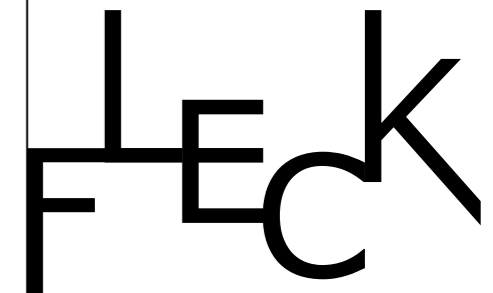
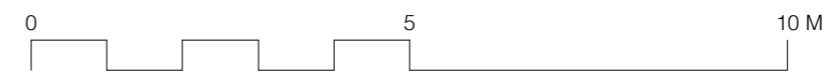


sectional elevation a-a

Page 85



sectional elevation b-b



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PROJECT:
Town houses, Pepys Road SW20

STATUS:
PLANNING

DRAWING:
Proposed elevations a-a & b-b

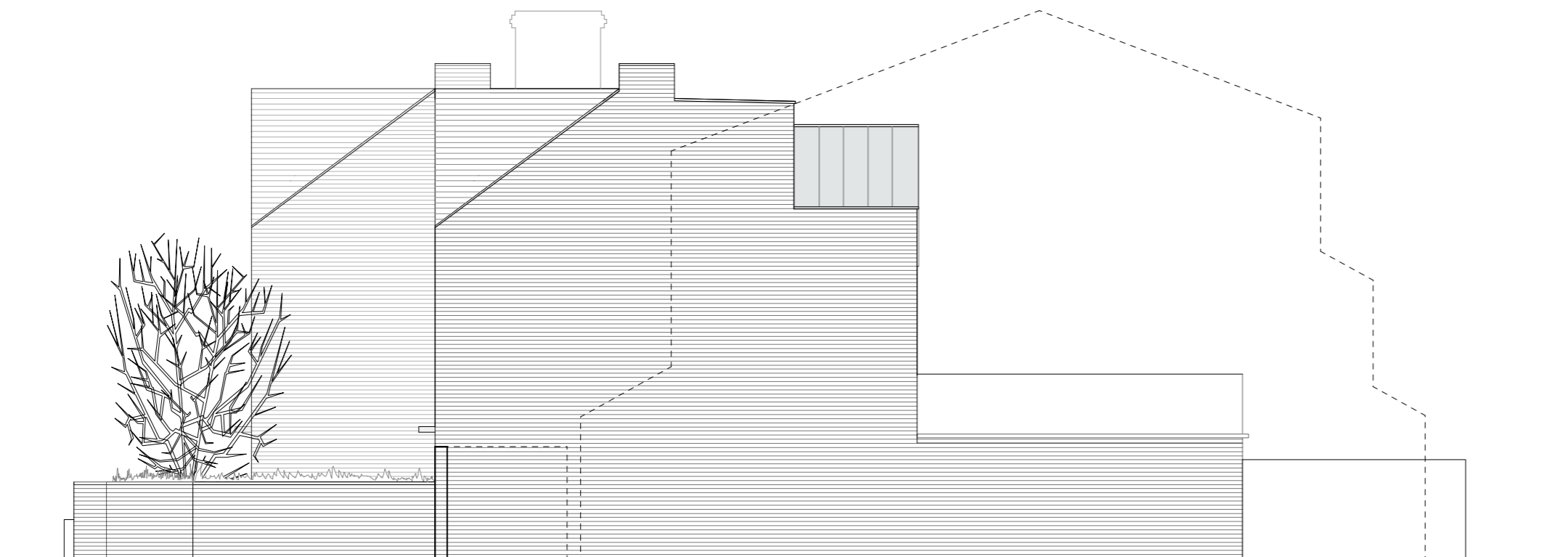
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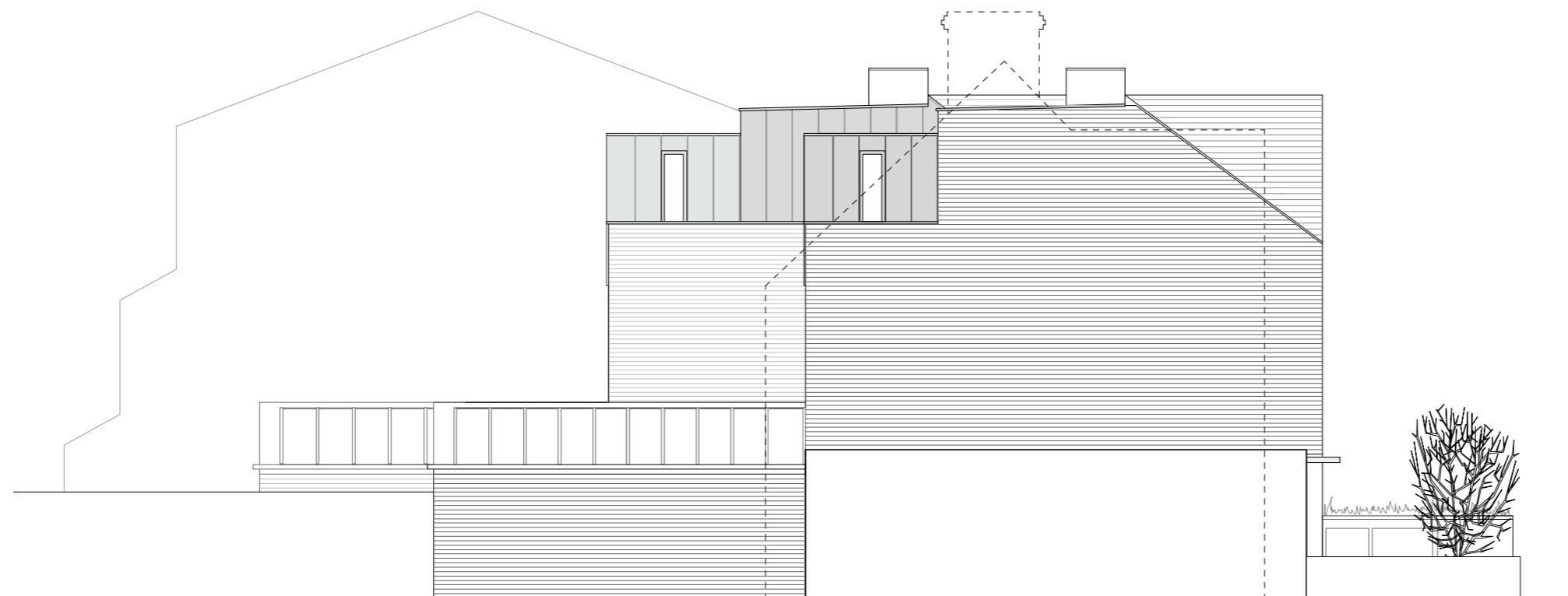
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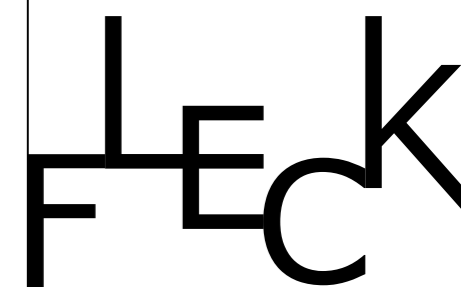
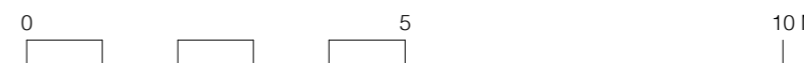
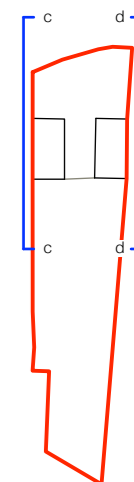
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south west side elevation c-c



north east side elevation d-d



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Town houses, Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed elevations c-c & d-d

DRAWN BY: DATE:

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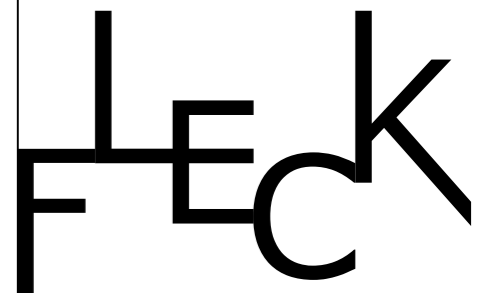
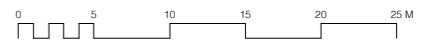
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PROJECT: _____

Pepys Road SW20

STATUS: _____

PRE-AP CONSULTATION

DRAWING: _____

SITE LOCATION PLAN

DRAWN BY: _____ DATE: _____

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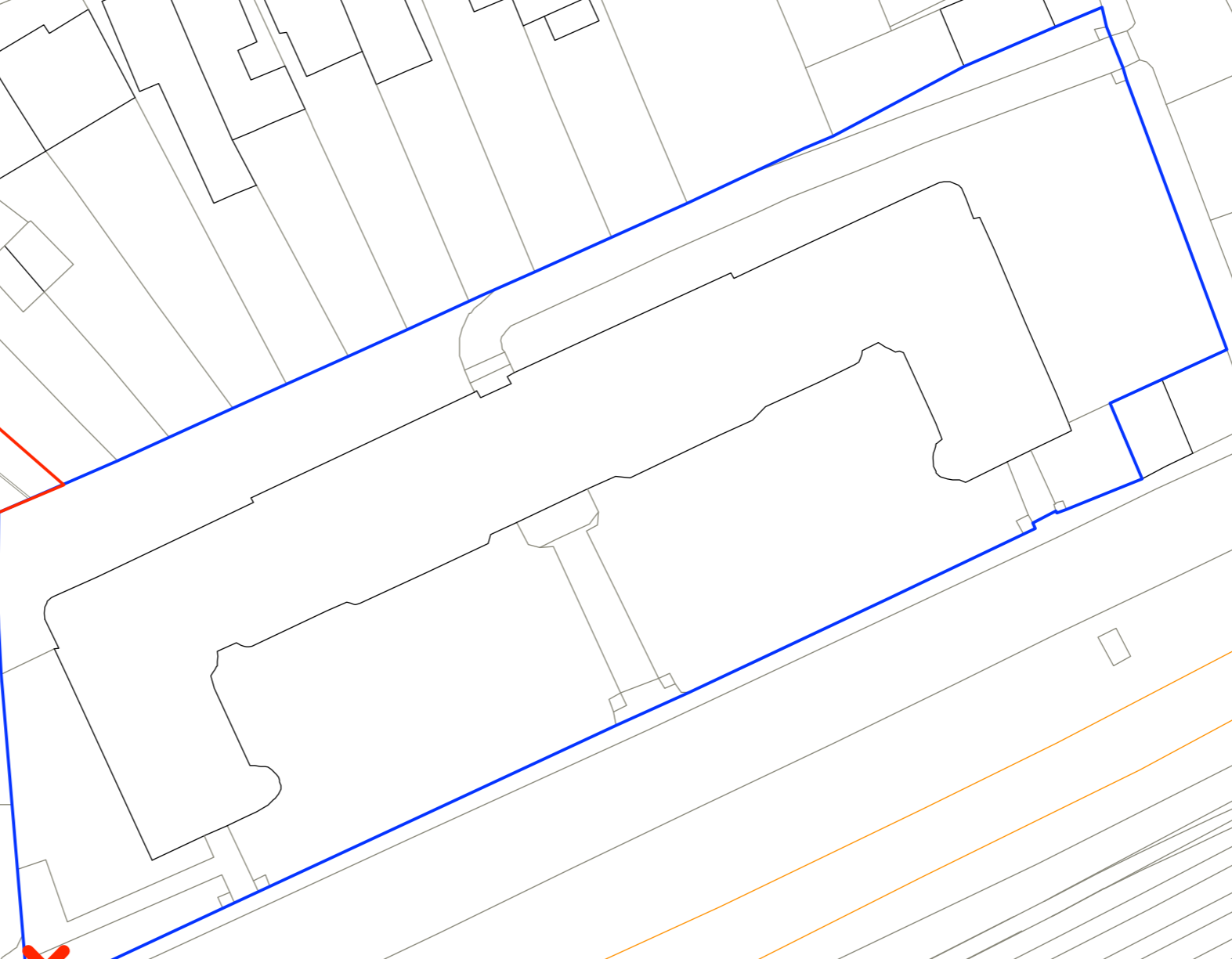
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Page 87

PEPYS ROAD

WYKE ROAD

potential location of electricity substation



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PLANNING APPLICATIONS COMMITTEE
28th April 2022.

UPRN	APPLICATION NO. 21/P3428	Item No: DATE VALID 08.10.2021
Address/Site	Electrical substation adjacent to 14 Pepys Road Raynes Park SW20 8NH	
Ward:	Raynes Park	
Proposal:	Demolition of existing electrical sub-station and the erection on site of 3 x self contained flats with external amenity space and access to grounds of Langham Court	
Drawing Nos	Site location plan and drawings 1158P- 20, 200, 201, 210, 211, 212 & 213	
Contact Officer:	Leigh Harrington (020 8545 3836)	

RECOMMENDATION

Grant planning permission subject to conditions and S106 agreement.

CHECKLIST INFORMATION.

- Heads of agreement: Permit Free
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No
- Design Review Panel consulted: No
- Number of neighbours consulted: 193
- Press notice – No
- Site notice – No
- External consultations: No
- Archaeological Priority Zone – No
- Controlled Parking Zone – Yes
- Number of jobs created: N/A
- Density 277 Habitable rooms per ha.

1 INTRODUCTION

- 1.1 The application has been brought before the Planning Applications Committee for consideration as it falls outside of the Scheme of Delegation due to officer recommendation of grant permission subject to conditions and the number of objections received from local residents.

2. SITE AND SURROUNDINGS

- 2.1 The application site is a parcel of land positioned along the eastern side of Pepys Road within Raynes Park, located between no.14 Pepys Road and no. 264 Worple Road. The site sits at the junction corner, being fronted by a zebra pedestrian crossing and adjoins Langham Court at the rear. The land is narrow and elongated, running in an approximate north-west to south-east direction with an average width of 8m and length of 40m. The land currently accommodates two single storey buildings serving as an electrical substation, with a gate fronting the site restricting access. An area of hard landscaping provides access from the street to the substation at the front with some soft landscaping and a tree. The rear is made up of soft landscaping with trees and hedges.
- 2.2 The site is situated just outside of Raynes Park Town Centre, with predominantly commercial properties to the west and residential to the north and east. No. 14 Pepys Road which adjoins to the south-west is a three storey office building whilst to the north-west No. 264 Worple Road is a two storey dwellinghouse with single storey garage that is built onto the substation.
- 2.3 The site is not located within a conservation area nor is it in anyway listed. The site is located within a controlled parking zone (Zone RPE) and has a public transport accessibility level (PTAL) of 5 (0 being the lowest and 6b being the best). The site is with in a very short walk of Raynes Park mainline rail station and a number of bus services. Cycleway C31 runs nearby and links to Kingston.

3. CURRENT PROPOSAL

- 3.1 The proposals form part of a series of four applications submitted for housing on the site, LBM Ref 20/P2583 is also for consideration by members at this meeting.
- 3.2 The proposal is for demolition of existing electrical sub-station and erection on site of 3 x self-contained flats with external amenity space and access to grounds of Langham Court.
- 3.3 The new block would have an offset footprint which would adjoin with the building line of the adjacent houses on the Worple Road side of the site. The front of this square bay would be set behind landscaping whilst the other half of the front elevation would set behind a hard standing area with cycle storage and provide the entrance to the block. A pathway

would run along the Pepys Road side of the site providing a foot access to Langham Court and the refuse stores would be located along this side entrance.

- 3.4 The entrance lobby serves the stairwell to the upper flats and entrance to the ground floor flat. The ground floor flat would have a combined kitchen/living dining room area to the front with rear doors to a patio area. A centrally positioned bathroom would be provided along with a single and double bedroom with access out to a green area to the rear of the patio. That rear bedroom would feature a flat roof with a green roof arrangement.
- 3.5 At first floor level the proposals would provide a second flat, the front of which would be within a continuation of the ground floor front footprint and with a similar internal layout although this unit would be a one-bedroom unit with doors from the bedroom leading out to an amenity terrace above the ground floor element.
- 3.6 The proposed second floor would provide a similar internal layout to the first floor flat and would again be a one-bedroom unit with doors from the bedroom leading out to an amenity terrace.
- 3.7 The roof would be finished in natural or fibre cement slate for the pitched roof with a green roof to the rear and would feature a front gable with sloping roof form to the front and side.
- 3.8 The block would be finished predominantly in exposed brick work, some of which would be in the form of decorative brickwork panels. Windows would be powder coated metal or composite metal/ timber. The doors would be timber with powder coated metal louvre doors to refuse store and slated timber doors to bicycle store.
- 3.9 To soften the appearance of the rear elevation it is proposed to provide climbing plants through a combination of direct greening and indirect greening rather than a 'living wall' system. Climbing plants will be planted adjacent to the wall at ground floor level. This will be supplemented by further climbers planted on the green roof. The planting would be irrigated using rainwater from the balcony and adjacent roof area. Stainless steel wires will be used to encourage growth between first and second floor levels and across the balustrades, the textured brickwork on the facade would offer a suitable substrate for the climbers to self attach elsewhere.
- 3.10 Proposed units

Unit	Type	Prop.GIA	Req. GIA	Prop amenity	Req Amenity
Flat 1	2B 3P	66m ²	61m ²	32m ²	6m ²
Flat 2	1B 2P	54m ²	50m ²	6m ²	5m ²
Flat 3	1B 2p	51m ²	50m ²	6m ²	5m ²

4. **PLANNING HISTORY**

- 4.1 20/P2583 Demolition of existing substation buildings and development of site to create 2 x 3 storey 3 bedroom dwellinghouses – Currently for determination by PAC
- 4.2 20/P2682 Demolition of existing electrical sub-station and erection on site of new 3 bed dwellinghouse with front and rear gardens and access to grounds of Langham Court - pending determination
- 4.3 21/P2784 Demolition of existing electrical sub-station and erection on site of new 4 bed dwellinghouse with front and rear gardens and access to grounds of Langham Court. - pending determination

5. **CONSULTATION**

- 5.1 Consultation letters sent to neighbours and site notice posted. Given the high level of previous objection consultation letters were sent to 193 residents. Objections were received from 113 residents raising concerns relating to;
- Fire Safety; the site is a Fire and Access service access route to the rear of the site. Fire Engines and crews would use this access.
 - Wyke Road is narrow and easily blocked and this would remove a potential access point.
 - Breach of lease agreements over pathways, gardens and accessways to and from the site.
 - Unfair to residents to build over their communal gardens for which they have a right of access.
 - The three storey height will result in loss of light and amenity to a number (10) of flats in Langham Court
 - By virtue of height and position would constitute a visually intrusive form of development
 - Loss of privacy to flats from the proposed balconies
 - The new flats will not benefit from good natural light because of the height of Langham Court.
 - It is within the green corridor and provides important wildlife habitat
 - It will remove two trees and support climate change
 - No affordable housing and just a profit for the freeholder
 - It will increase risk of flooding, floods have caused the boiler room to break down
 - The developer is just being greedy.
 - New residents would have uncontrolled access to Langham Courts gardens and refuse facilities
 - Devalue the flats and leaseholds

- The junction is to be remodelled and laid out in the future and there should be no infill development until that work is completed.

5.2 Flood Risk officers raised no objections

5.3 Transport Planning officers raised no objections

5.4 Tree Officer - The Council's Arboricultural officer commented that whilst there were no arboricultural objections seen to the scheme, the arboricultural reports should either be amended to incorporate the site visits, or attach conditions F5 & F8.

5.5 London Fire Brigade

The local fire service Station Officer undertook an initial site visit and commented that the access point is needed and would be used in the event of an incident taking place at the flats. The local Fire Station Officer Comments;

8/11/2021

"Sorry for the delay in responding. As discussed I visited the site after your visit. From the walk around that we did I would say that the access is needed and would be used in the event of an incident taking place at the flats. Although it would be possible at the moment to manoeuvre a fire appliance down the access route to the flats it would not be something that we would do due to the lack of hard standing and the close proximity of the road. It would be necessary however to retain some form of access so personnel with equipment could access the rear of the flats on foot. I should stress however that this is my opinion and if you need official documentation this can be arranged by our fire safety department. I have contacted them and am just waiting for their reply. I would assume they would take my assessment as the brigade stance unless there is some legislation that applies. I will forward their reply to you as soon as I have it".

Officers sought further clarification as to whether the Fire Brigade Safety Department had any comments to make and the Station officer replied on

10/1/2022

I'm very sorry about the delay in getting back to you on this enquiry. I have managed at last to pin down our Fire Safety department. Unfortunately they can give no further insight into the application outside of what I said a couple of months back. This is because it is a planning application issue and as such they will

not comment on it. So from our perspective I can only reiterate what I stated before:

‘access is needed and would be used in the event of an incident taking place at the flats. Although it would be possible at the moment to manoeuvre a fire appliance down the access route to the flats it would not be something that we would do due to the lack of hard standing and the close proximity of the road. It would be necessary however to retain some form of access so personnel with equipment could access the rear of the flats on foot’.

Given the presence of other access points around Langham Court officers sought further clarification from the officer on this point and on

27/02/2022

“Sorry for the delay in coming back to you. In answer to your question I don’t think that this is a fire safety issue, as confirmed by our fire safety department. The reason for the opinion I gave was from a practical operational perspective. It could be better to have rear access to the flats for faster deployment of resources and the ability for a dual approach. However, if that wasn’t available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. This would then be the same for many other places where there is only front access.”

5.6 UK Power Networks

The company were consulted on the proposals as they would be involved in the removal of their electricity substations. It was confirmed that the stations are operational and whilst it would be physically possible to relocate them this would involve separate applications to them.

6. POLICY CONTEXT

Relevant policies in the London Plan 2021 are;

H1 (Increasing housing supply)

H2 (Small sites)

D1 (London’s form, character and capacity for growth)

D3 (Optimising site capacity through a design lead approach)

D5 (Inclusive design)

D6 (Housing Quality and standards)

D11 (Safety & Security)

D12 (Fire safety)

GG2 (Making the best use of land)

GG4 (Delivering Homes Londoners need)

GG6 (Increasing efficiency and resilience)

SI 2 (Minimising greenhouse gas emissions)

SI.3 (Sustainable drainage)
SI. 5 (Water infrastructure)
S1.7 (Reducing waste and supporting the circular economy)
S1.8 (Waste capacity and net waste self-sufficiency)
T 5 (Cycling)
T6.1 (Residential Parking),

Merton Local Development Framework Core Strategy (2011)
CS 8 Housing choice.
CS 9 Housing targets
CS 11 Infrastructure
CS 13 Open space and nature conservation
CS 14 Design
CS 15 Climate Change
CS 17 Waste
CS 18 Transport
CS 20 Parking servicing and delivery

The relevant policies in the Council's Adopted Sites and Policies Plan 2014 are:

DM D1 Urban design and the public realm
DM D2 Design considerations
DM O2 Nature conservation, trees and hedges
DM T2 Transport impacts of development
DM T3 Car Parking and servicing standards

NPPF 2021

London Housing Supplementary Planning Guidance 2016.

7. PLANNING CONSIDERATIONS

7.1 The principal planning considerations in this case relate to the principle of residential development, fire safety, electricity supply infrastructure, design, the impact on neighbour amenity, standard of residential accommodation and trees.

7.2 Principle of development and the need for housing

7.2.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

7.2.2 The National Planning Policy Framework 2021, London Plan 2021 policy H1 (Increasing housing supply) and the Council's Core Strategy policy CS9 all seek to increase sustainable housing provision where it can be shown that an acceptable standard of accommodation will also provide a mix of dwelling types. Policy D3 of the London Plan 2021 requires all development to make the best use of land by following a design-led

approach that optimises the capacity of sites, including site allocations. Optimising site capacity means ensuring that development is of the most appropriate form and land use for the site.

- 7.2.3 The National Planning Policy Framework and London Plan policies H1 and H2 (Small sites) promote sustainable development that encourages the development of additional dwellings at locations with good public transport accessibility. This site is within ready walking distance of Raynes Park station and a number of bus routes. Core Strategy policies CS8 & CS9 seek to encourage proposals for well-designed and conveniently located new housing that will create socially mixed and sustainable neighbourhoods through physical regeneration and effective use of space. The principle of residential development is therefore considered acceptable.
- 7.2.4 Policy H1 of the London Plan 2021 has set Merton a ten-year housing target of 9,180 new homes. By providing 3 new units the proposals would make a small but important contribution to meeting that target and providing much needed new housing. The proposal to introduce residential use to this site is considered to respond positively to London Plan and Core Strategy planning policies to increase housing supply, optimise sites and is therefore supported by Officers.

7.3 **Fire Safety**

- 7.3.1 London Plan 2021 policy D12 (Fire safety), is concerned with ensuring that new developments are not at risk from fire. It requires developments to provide suitable and convenient means of escape, and associated evacuation strategy for all, to identify suitably positioned unobstructed outside space and provide access for fire service personnel and equipment. Whilst it is acknowledged that the policy is aimed at primarily at ensuring fire safety at new developments it is considered relevant in ensuring that new developments do not impact on fire safety at other developments.
- 7.3.2 All the pending planning applications on the application site have all been accompanied by a high number of objections from the residents of Langham Court, which is a large block of flats to the rear of the site, concerned that the use of the site will hamper Fire Brigade access to them in the event of emergency.
- 7.3.4 As part of the application process, officers consulted the local London Fire Brigade Station Officer (full details in section 5.5 of this report) who, after initial reservations confirmed on the 27/02/2022 (following officer clarification in regard to the presence of other access points around Langham Court) that:

“Sorry for the delay in coming back to you. In answer to your question I don’t think that this is a fire safety issue, as confirmed by our fire safety department. The reason for the opinion I gave

was from a practical operational perspective. It could be better to have rear access to the flats for faster deployment of resources and the ability for a dual approach. However, if that wasn't available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. This would then be the same for many other places where there is only front access."

7.3.5 As set out above, the London Fire Brigade raise no fundamental objection to the proposed scheme. They confirmed that they don't think this is a fire safety issue, could be better to have rear access, however if that wasn't available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. Therefore, based on the advice received, officers consider that there would be limited justification to refuse planning permission on fire safety grounds.

7.3.6 As a high number of local residents raised concerns with the very serious matter of fire safety, officers sought additional information and clarification from the applicant on this point. The applicant was therefore required to provide an independent fire strategy report with the application. Such reports are only usually required for major development however given the concerns raised by local residents, officers made a request for a fire strategy report in this instance. The applicants have submitted a Fire Strategy report compiled by R.Bosdet DMS, MIFireE (Member of the Institution of Fire Engineers) of Phoenix Executive Fire Advisory Associates that confirms that the existing access arrangements are adequate and that the proposed residential use of the site would not impact fire safety for the residents of Langham Court.

7.3.7 If planning permission is granted by members of the planning committee, it should be noted that the site of the flats at Langham Court can still be accessed along both Langham Road and Wyke Road by vehicles and pedestrians.

7.3.8 In conclusion, officers have proactively sought the views of professionals in terms of fire risk. Both the London Fire Brigade and the applicants independent fire expert have confirmed no fundamental objections or concerns with fire safety. Therefore, based on the information before officers, officers are content that the proposals would not result in an unacceptable increase in risk to the safety of Langham Court Residents in the event of fire to justify a refusal of planning permission. It should also be noted that the new buildings would be subject of separate Building Regulations approvals process which addresses fire safety matters.

7.4 **Electricity supply infrastructure**

7.4.1 Core strategy policy CS 11 (Infrastructure) states out that the Council will work with utility suppliers to ensure the safe and secure transportation of utilities such as electricity is not compromised.

7.4.2 UK Power Networks have confirmed that the substation is an active facility but have raised no objection to the application. UK Power Networks have not confirmed if the removal of the substation would result in electricity being compromised, but it is assumed that alternative facilities would need to be provided before this site was deactivated. The applicant has indicated that alternative provision could be provided within the site near to the refuse facilities associated with Langham Court in the further southwestern corner of the Langham Court land. However, this is only indicative and has not been agreed with UK Power Networks as being a feasible option.

7.4.3 In order to ensure that there are no negative impacts on local electricity supply it is recommended that a condition be attached that requires confirmation that the applicant has got the necessary approvals from UK Power Networks and that no development shall be undertaken until alternative electrical substation facilities have been provided and are operational.

7.5 **Design**

7.5.1 London Plan 2021 policy D3 (Optimising site capacity through a design lead approach), Core Strategy policy CS14 (Design) and SPP policy DM D2 (Design considerations) require developments to relate positively and appropriately to the siting, scale, proportions and massing of surrounding buildings and existing street patterns and which would enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions.

7.5.2 The site has an industrial character from the functional substation structures and is located on a bend in the road between a terrace of houses along Worple Road and a more modern office block on Pepys Road. This transitional nature of the site is considered to allow for a greater diversity of design options than would be the case otherwise. The proposed design is however considered attractive and reflect the height and front gable feature of the front elevations of the adjacent houses and the materials of the offices.

7.5.3 Conditions requiring details of materials to be approved and restricting the provision of cables, wires, aerials, pipe work on the front elevation are recommended to ensure a satisfactory appearance for the development and to guard against value engineering. Subject to those factors the appearance and character of the houses are considered acceptable.

7.6 **The impact on neighbour amenity**

7.6.1 SPP policy DM D2 and London Plan policy D3 require that proposals do not have a negative impact on neighbour amenity from loss of light, privacy, visual intrusion or increased disturbance and that people feel comfortable with their surroundings. The proposals have generated objections on the grounds of amenity including a loss of light to neighbouring properties.

Langham Court

7.6.2 The occupiers of a number of the flats in Langham Court objected that they would suffer from a loss of light although those flats are nearly 30m from the proposed rear boundary fence. Given this distance and the orientation of the site to those flats it is considered that little weight can be afforded those objections or the building appearing visual intrusive. In terms of overlooking and impact on privacy, whilst it is acknowledged that the screening walls to the upper floor terraces would not restrict views towards Langham Court those flats are some 30m away from the proposed amenity terraces which would ensure that there is no undue loss of amenity.

284 Worple Road

7.6.3 The design of the proposed block of flats would have a staggered rear footprint. The recessed rear element of the proposed building closest to this neighbour would align with its existing garage/rear elevation. Whilst the remaining part of the proposed flats would project beyond the rear building line of this neighbouring house (6.2m at ground floor and 3.7m projection at the upper levels), this part of the proposed building and its flank wall would be inset between 2.2m and 2.7m away from the boundary. Therefore, given the level of separation from the boundary, there would remain a suitable level of breathing space from this neighbouring property.

7.6.4 It is noted that there would be some impact on light levels, given that the proposed block of flats would sit to the south of this neighbour, however as stated above, the block of flat at the upper levels only projects 3.7m beyond the rear of the neighbour and its flank wall would be inset from the boundary by at least 2.2m. It should also be noted that the existing office block (14 Pepys Road) would already cast a degree of shadowing over the neighbours garden. It is therefore considered that there would be no undue loss of light to warrant refusal of planning permission.

7.6.5 Objections were raised concerning overlooking and loss of privacy from the two upper floor amenity areas. The proposed terraces are set away from the boundary and the plans show screening walls to restrict side views over the adjoining garden at 284 Worple Road. It is therefore considered that there would be no undue overlooking or loss of privacy.

14 Pepys Road

- 7.6.6 This site is a commercial premises of offices and therefore no undue loss of amenity in anticipated.

Conclusion (amenity)

- 7.6.7 In view of the these factors the proposals are not considered to be materially harmful to the amenity of neighbouring occupiers and therefore to be acceptable in this regard.

7.7 **Standard of Residential accommodation**

SPP Policy DM D2, Core Strategy 2011 policies CS 9 & CS 14 and London Plan policy D6 (Housing standards) are all policies that seek to provide additional good quality residential accommodation with safe and secure access that meets minimum internal and external space standards.

7.7.1 Accommodation schedule

Unit	Type	Prop.GIA	Req. GIA	Prop amenity	Req Amenity
Flat 1	2B 3P	66m ²	61m ²	32m ²	6m ²
Flat 2	1B 2P	54m ²	50m ²	6m ²	5m ²
Flat 3	1B 2p	51m ²	50m ²	6m ²	5m ²

- 7.7.2 All three flats are dual aspect and offer a well set out range of rooms with good levels of natural light and they meet all exceed both the relevant internal GIA and exterior amenity space standards for properties of this size. The proposed accommodation is consequently considered acceptable.

7.8 **Parking, servicing and deliveries**

Planning Policy T1 (Strategic approach to transport) of the London Plan 2021 states that the delivery of the Mayor's strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041. All development should make the most effective use of land, reflecting its connectivity and accessibility by existing and future public transport, walking and cycling routes, and ensure that any impacts on London's transport networks and supporting infrastructure are mitigated.

- 7.8.1 Planning Policy DM T2 (Transport impacts of development) of Merton's Sites and Policies Plans seeks to ensure that development is sustainable and has minimal impact on the existing transport infrastructure and local environment.

Cycle parking

- 7.8.2 Planning Policy T5 (Cycling) of the London Plan 2021 states that development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle. Developments should provide cycle parking at least in accordance with the minimum standards set out in Table 10.2. In accordance with Table 10.2, residential dwellings should provide 1 space per studio/1 person 1 bedroom dwelling, 1.5 spaces per 2 person 1 bedroom dwelling and 2 spaces per all other dwellings.
- 7.8.3 A store for the block is proposed to the front of the site to accommodate the required 5 cycles and there is scope for additional space to be provided at the rear of the access path. There would also be tubular mounts for visitor/short term secure facilities.

Car parking

- 7.8.4 Planning Policy T6 (Car parking) of the London Plan 2021 states that Carfree development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Car-free development has no general parking but should still provide disabled persons parking in line with standards set out in policy T6.1 (Residential parking). Planning Policy DM T3 (Car parking and servicing standards) of Merton's Sites and Policies Plan also promotes car free development in areas which benefit from good access to public transport (generally PTAL 4 or above) and be within an existing controlled Parking Zone.
- 7.8.5 The application site has a PTAL rating of 5 (good) and within Controlled Parking Zone RPE. Therefore, the development is considered to be suitable as a permit free development as this would take pressure off existing car parking zones and promote sustainable modes of transport. The applicant has agreed to the permit free requirement, this can be controlled via a S106 agreement restricting permits.

7.9 Bin and Recycling Storage

- 7.9.1 Planning Policy D6 (Housing quality and standards) of the London Plan 2021 states that housing should be designed with adequate and easily accessible storage space that supports the separate collection of dry recyclables (for at least card, paper, mixed plastics, metals, glass) and food waste as well as residual waste.
- 7.9.2 A communal refuse bin storage area is proposed to the front/side of the block, which is considered a suitable and accessible location with refuse facilities of an appropriate size for the development. Therefore the proposal complies with policy CS17(Waste) of the council's Core Strategy Policy and policies SI 7 (Reducing waste and supporting the

circular economy) and SI 8 (Waste capacity and net waste self-sufficiency) of the London Plan 2021 although a condition requiring details of the design to be approved is recommended to ensure suitability of appearance and functionality.

8. SUSTAINABLE DESIGN AND CONSTRUCTION

- 8.1 Merton's Core Planning Strategy (2011) Policy CS15 (Climate change) outlines how all minor and major development, including major refurbishment, should demonstrate: how the proposal makes effective use of resources and materials, minimises water use and CO2 emissions; makes the fullest contribution to minimising carbon dioxide emissions in accordance with the energy hierarchy and designed to withstand the long term impacts of climate change.
- 8.2 London Plan Policies SI 2 (Minimising greenhouse gas emissions) & SI 5 (Water infrastructure) and Merton's Sustainable Design and Construction Explanatory note, expect developments to achieve carbon reductions beyond Part L from energy efficiency measures alone to reduce energy demand as far as possible.
- 8.3 For minor residential developments, development is required to achieve a 19% improvement on Part L of the Building Regulations 2013 and water consumption should not exceed 105 litres/person/day.
- 8.4 The application was submitted with a sustainable design and construction statement outlining how the development has been designed in order to meet these requirements for energy and water savings. It is recommended that a condition be attached requiring details be submitted to demonstrate that a sustainable building has been provided in accordance with relevant policy.

9. Trees

- 9.1 The applications were accompanied by Arboricultural Impact Assessments which confirmed that the proposals would involve the removal of three trees (A Cat B Sycamore, a Cat C Walnut and Cat C Elder) and a shrub (Privet hedge). Some basic tree protection measures and working methodology (in accordance with BS 5837:2012) will ensure the retained and third-party trees and hedges are not detrimentally affected during construction. The Councils Tree Officer has confirmed no objection to the proposal subject to conditions.
- 9.2 Whilst the details would be secured by condition the AIA has suggested the planting of two Silver Birch to the front of the site along with landscaping and another tree to the rear. The new trees will be of standard size (about 2-3m high), planted in full accordance with current British Standards (BS 8545: From Nursery to Independence in the Landscape) and once planted, the trees will be regularly maintained (watered and weeded during the spring and summer months) for at least

5 years or until established. On balance, whilst there would be the removal of three trees, the grade of the trees is low and offers limited amenity value in this instance. In that assessment is the fact that new tree planting can be secured via planning condition.

10. **Other matters**

- 10.1 The existing site is partially open to wildlife but it is not designated as any form of 'green' amenity facility and therefore its loss would not constitute grounds for refusal.
- 10.2 The proposals would impact on the garage of the adjoining neighbour and had the application been recommended for approval it would have been conditioned such that details of how the sub stations could be demolished with no ill effect on that neighbouring structure would be required to be approved and would be subject to the agreement of that neighbour.
- 10.3 The area is at low risk of flooding and therefore development could not be refused on the principle of a development at this location.
- 10.4 Building on land within the demise of Langham Court would be a civil matter between the leaseholders and freeholder and the impact on the value of nearby properties is similarly not a planning matter.
- 10.5 The proposal is for three flats and therefore does not fall subject to the threshold for being subject to affordable housing contributions/provision

11. **CONCLUSION**

- 11.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.
- 11.2 The development of the site would provide new housing for which there is a recognised need. The block of three flats is considered to be well and attractively designed and exceed the minimum requirements for internal and external space. The bulk scale, massing and siting of the block is considered to be of an acceptable appearance and not to have a negative impact on neighbour amenity.
- 11.3 There are considered to be no justifiable reasons to refuse the application on the grounds of fire safety and subject to the provision of an alternative electricity substation prior to the demolition of the existing buildings the proposals will not negatively impact utility supplies.
- 11.4 The site is ideally located in Raynes Park town centre with its local facilities and very good public transport connections and therefore is

suitable for being a permit free development in order to reduce reliance on the car and to increase pressure on parking locally.

- 11.5 The proposal are therefore considered to be in accordance with Adopted Sites and Policies Plan, Core Planning Strategy and London Plan policies. The proposal is therefore recommended for approval subject to conditions and S106 agreement.

12. RECOMMENDATION

GRANT PLANNING PERMISSION subject to –

1. Subject to conditions and the completion of a Section 106 Agreement covering the following heads of terms:-

1. Permit Free.
2. The developer agreeing to meet the Councils costs of preparing, drafting and monitoring the Section 106 Obligations.

And the following conditions:

1. A1 Commencement of Development
2. A7 Approved Plans; Site location plan and drawings, 1158P- 20, 200, 201, 210, 211, 212 & 213
3. No development shall commence until alternative electrical substation facilities, as confirmed by UK Power Networks, to serve the local area have been provided and made fully operational before any works have commence on site.

Reason, to ensure the continuous provision of electricity utilities in accordance with policy CS11 of the Merton Core Strategy 2011.

4. B1 External Materials to be Approved – No development, other than demolition, shall take place until details of particulars and samples of the materials to be used on all external faces of the development hereby permitted, including window frames and doors (notwithstanding any materials specified in the application form and/or the approved drawings), have been submitted to the Local Planning Authority for approval. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall be carried out in full accordance with the approved details.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for

Merton: policies D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

5. C07 Refuse & Recycling – No development shall be occupied until details a refuse management plan for the management and collection of waste on site have been submitted in writing for approval to the Local Planning Authority.

Reason: To ensure the provision of satisfactory management methods for the storage and collection of refuse and recycling material and to comply with the following Development Plan policies for Merton: policies SI 7 and D6 of the London Plan 2021, policy CS17 of Merton's Core Planning Strategy 2011 and policy DM D2 of Merton's Sites and Policies Plan 2014.

6. D10 External Lighting – Any external lighting shall be positioned and angled to prevent any light spillage or glare beyond the site boundary.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policies DM D2 and DM EP4 of Merton's Sites and Polices Plan 2014.

7. D11 Construction Times – No demolition or construction work or ancillary activities such as deliveries shall take place before 8am or after 6pm Mondays - Fridays inclusive, before 8am or after 1pm on Saturdays or at any time on Sundays or Bank Holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policy D14 of the London Plan 2021 and policy DM EP2 of Merton's Sites and Polices Plan 2014.

8. Non-standard condition (Demolition and Construction Method Statement) – No development shall take place until a Demolition and Construction Method Statement has been submitted to, and approved in writing by, the Local planning authority. The approved Statement shall be adhered to throughout the demolition and construction period.

The Statement shall provide for:

- hours of operation
- the parking of vehicles of site operatives and visitors
- loading and unloading of plant and materials
- storage of plant and materials used in constructing the development

- the erection and maintenance of security hoarding including decorative -displays and facilities for public viewing, where appropriate
- wheel washing facilities
- measures to control the emission of noise and vibration during construction.
- measures to control the emission of dust and dirt during construction/demolition
- a scheme for recycling/disposing of waste resulting from demolition and construction works

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

9. H13 Construction Logistics Plan to be submitted – Prior to the commencement of the development hereby permitted, a Demolition/Construction Logistics Plan (including a Construction Management plan in accordance with TfL guidance) shall be submitted to and approved in writing by the Local Planning Authority. The approved Statement shall be adhered to throughout the construction period unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

10. F1 No development shall take place until full details of a landscaping and planting scheme has been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved before the commencement of the use or the occupation of any building hereby approved, unless otherwise agreed in writing by the Local Planning Authority. The details shall include on a plan, full details of the size, species, spacing, quantities and location of proposed plants, together with any hard surfacing, means of enclosure, and indications of all existing trees, hedges and any other features to be retained, and measures for their protection during the course of development.

11. F5 No development [including demolition] pursuant to this consent shall commence until an Arboricultural Method

Statement and Tree Protection Plan, drafted in accordance with the recommendations and guidance set out in BS 5837:2012 has been submitted to and approved in writing by the Local Planning Authority and the approved details have been installed. The details and measures as approved shall be retained and maintained, until the completion of all site operations.

12. F8 Site supervision: The details of the Arboricultural Method Statement and Tree Protection Plan shall include the retention of an arboricultural expert to supervise, monitor and report to the LPA not less than monthly the status of all tree works and tree protection measures throughout the course of the construction period. At the conclusion of the construction period the arboricultural expert shall submit to the LPA a satisfactory completion statement to demonstrate compliance with the approved protection measures.
13. B4 No development shall take place until details of the surfacing of all those parts of the site not covered by buildings or soft landscaping, including any parking, service areas or roads, footpaths, hard and soft have been submitted in writing for approval by the Local Planning Authority. No works that are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details have been approved and works to which this condition relates have been carried out in accordance with the approved details.
14. C5 No cables, wires, aerials, pipe work (except any rainwater down pipes as may be shown on the approved drawings) meter boxes or flues shall be fixed to any elevation facing a highway.
15. B5 No development shall take place until details of all boundary walls or fences are submitted in writing for approval to the Local Planning Authority. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details are approved and works to which this condition relates have been carried out in accordance with the approved details. The walls and fencing shall be permanently retained thereafter.
16. H07 Cycle Parking – No development shall not be occupied until the secure cycle parking facilities for the occupants of, and visitors to, the development shown on the approved drawings have been fully implemented and made available for use prior

to the first occupation of the development and thereafter retained for use at all times.

Reason: To ensure satisfactory facilities for cycle parking are provided and to comply with the following Development Plan policies for Merton: policy T5 of the London Plan 2021, policy CS18 of Merton's Core Planning Strategy 2011 and policy DM T1 of Merton's Sites and Policies Plan 2014.

17. Nonstandard condition; Sustainable build; The residential units hereby approved shall not be occupied until it has demonstrated to and approved in writing by the Local Planning Authority that they have achieved an energy efficiency standard of a minimum of 19% CO2 improvement over Building Regulations requirements Part L 2013 (TER Baseline) and, as a minimum, a water efficiency standard of not more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply with the following Development Plan policies for Merton: policy SI 2 of the London Plan 2021 and policy CS15 of Merton's Core Planning Strategy 2011.

18. C9 The screening or enclosure to the balconies as shown on the approved plans shall be implemented before the development is first occupied and retained permanently thereafter.
19. B6 No development shall take place until details of the proposed finished floor levels of the development, together with existing and proposed site levels, have been submitted to and approved in writing by the Local Planning Authority, and no development shall be carried out except in strict accordance with the approved levels and details.
20. F9 The hardstanding hereby permitted shall be made of porous materials, or provision made to direct surface water run-off to a permeable or porous area or surface within the application site before the development hereby permitted is first occupied or brought into use.
21. C6 No development shall take place until a scheme for the storage of refuse and recycling has been submitted in writing for approval to the Local Planning Authority. No works which

are the subject of this condition shall be carried out until the scheme has been approved, and the development shall not be occupied until the scheme has been approved and has been carried out in full. Those facilities and measures shall thereafter be retained for use at all times from the date of first occupation.

24. No development (other than demolition) approved by this permission shall be commenced until a scheme for the provision of surface and foul water drainage has been implemented in accordance with details that have been submitted to and approved in writing by the LPA. Before these details are submitted an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system (SuDS) to ground, watercourse or sewer in accordance with drainage hierarchy contained within the London Plan Policy SI.3 and the advice contained within the National SuDS Standards. Where a sustainable drainage scheme is to be provided, the submitted details shall:
- i. provide information about the design storm period and intensity, the method employed to delay (attenuation provision of no less than 13m³) and control the rate of surface water discharged from the site to greenfield runoff rates (no more than 5l/s), and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - ii. include a timetable for its implementation; and
 - iii. provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption authority and any other arrangements.

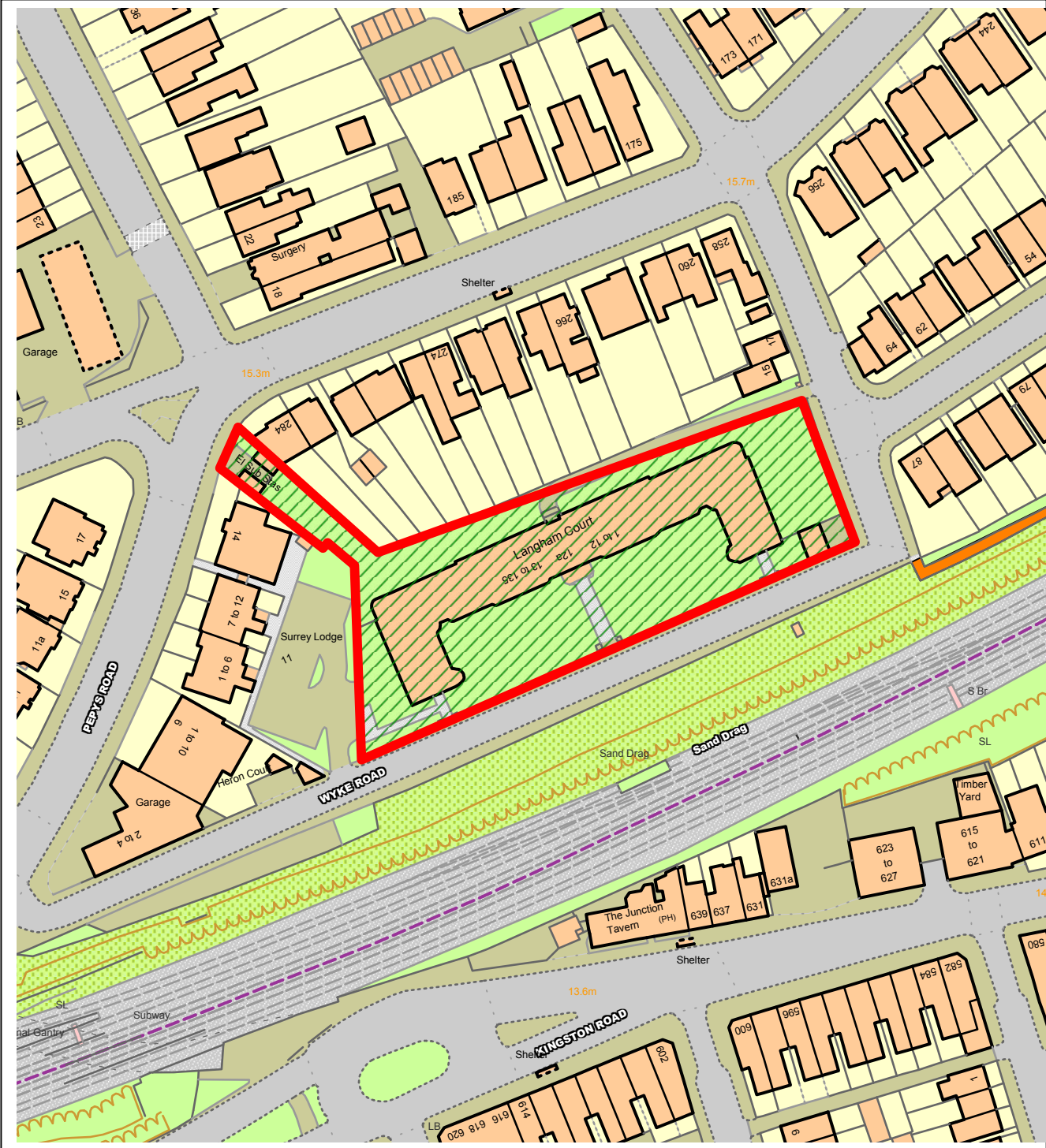
Reason: To reduce the risk of surface and foul water flooding and to ensure the scheme is in accordance with the drainage hierarchy of London Plan policy SI.3 and the National SuDS standards and in accordance with policies CS16 of the Core Strategy and DMF2 of the Sites and Policies Plan.

25 Green Roofs/Walls

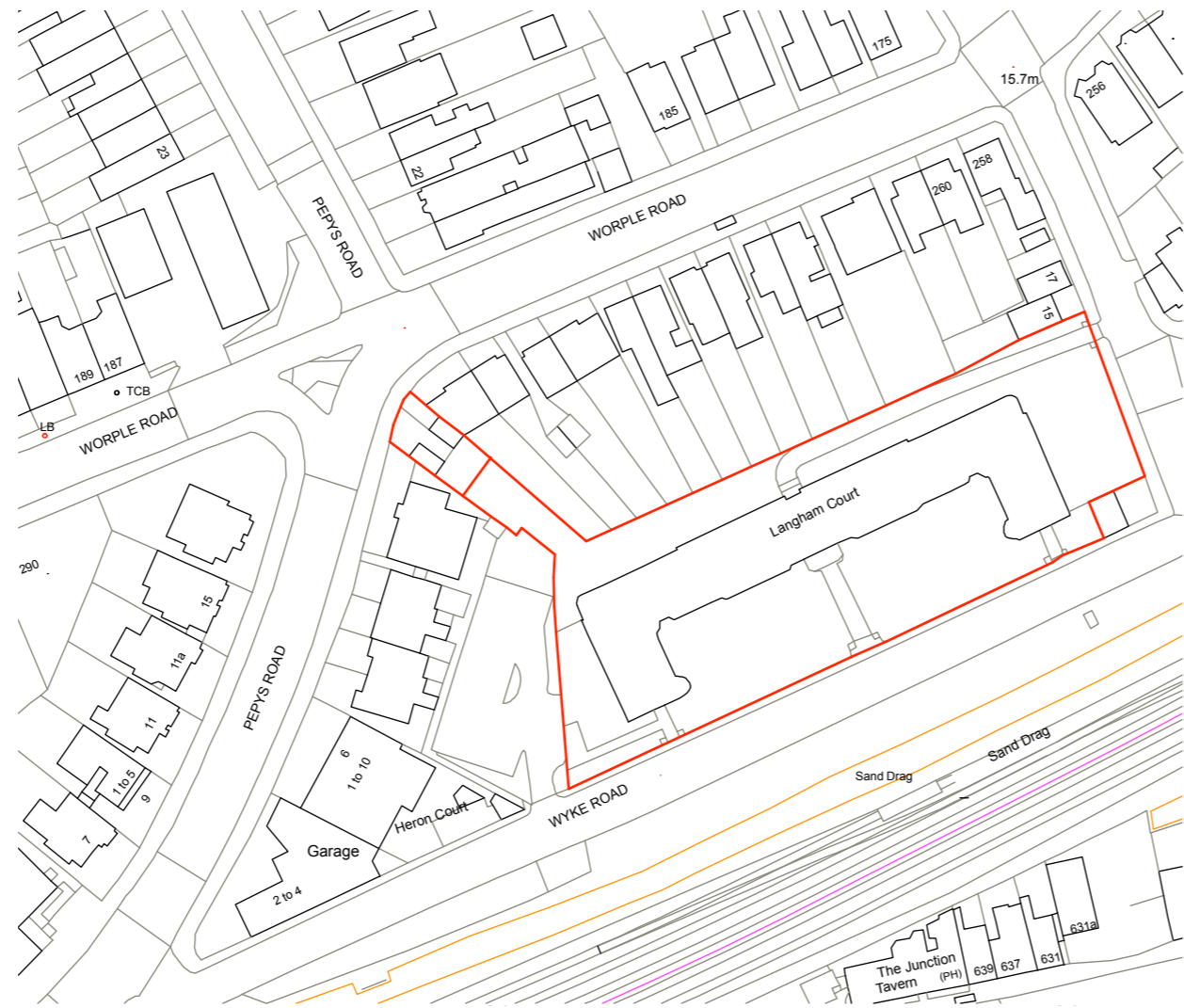
Prior to commencement of above ground works details for the proposed green roof and walls shall be submitted to an approved in writing by the Local Planning Authority and those approved measures shall be installed in the planting season following construction of the development.

Reason: To ensure the provision of an attractive and viable green roof and wall system to improve biodiversity and greening of the development in accordance with Merton sites and Policies Plan policies DM O2 and DM D2.

NORTHGATE SE GIS Print Template

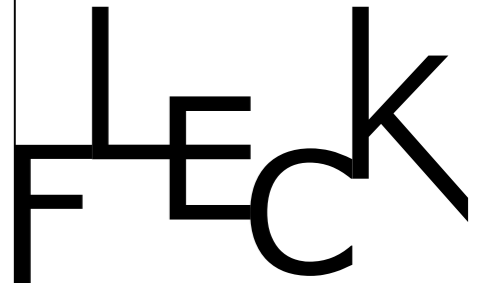


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24 Godwin Road, London E7 0LE
 T: 07814734654 E: info@fleckarchitecture.com

CLIENT: _____

A Azouz

PROJECT: _____

Substation site flats, Pepys Road
 SW20

STATUS: _____

PLANNING

DRAWING: _____

Site location plan

DRAWN BY: _____ DATE: _____

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pedestrian access route for Langham Court paving with permeable joints laid to fall toward planting areas

bicycle store with green roof and electric charging point

planting areas containing native planting helping to slow and reduce surface water run off and improve biodiversity of site

paving with permeable joints laid to fall toward planting area/ gravel

semi detached house at 284 Worple Road

garage belonging to 284 Worple Road

paving with permeable joints laid to fall toward planting areas

office building at 14 Pepys Road

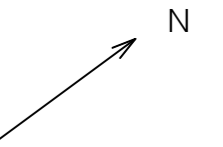
existing garden space unaffected by the proposals

existing planting along boundary with 284 Worple Road to be retained

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WB rainwater harvesting (water butt)



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CLIENT:

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PROJECT:

Substation site flats, Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed block plan

DRAWN BY: DATE:

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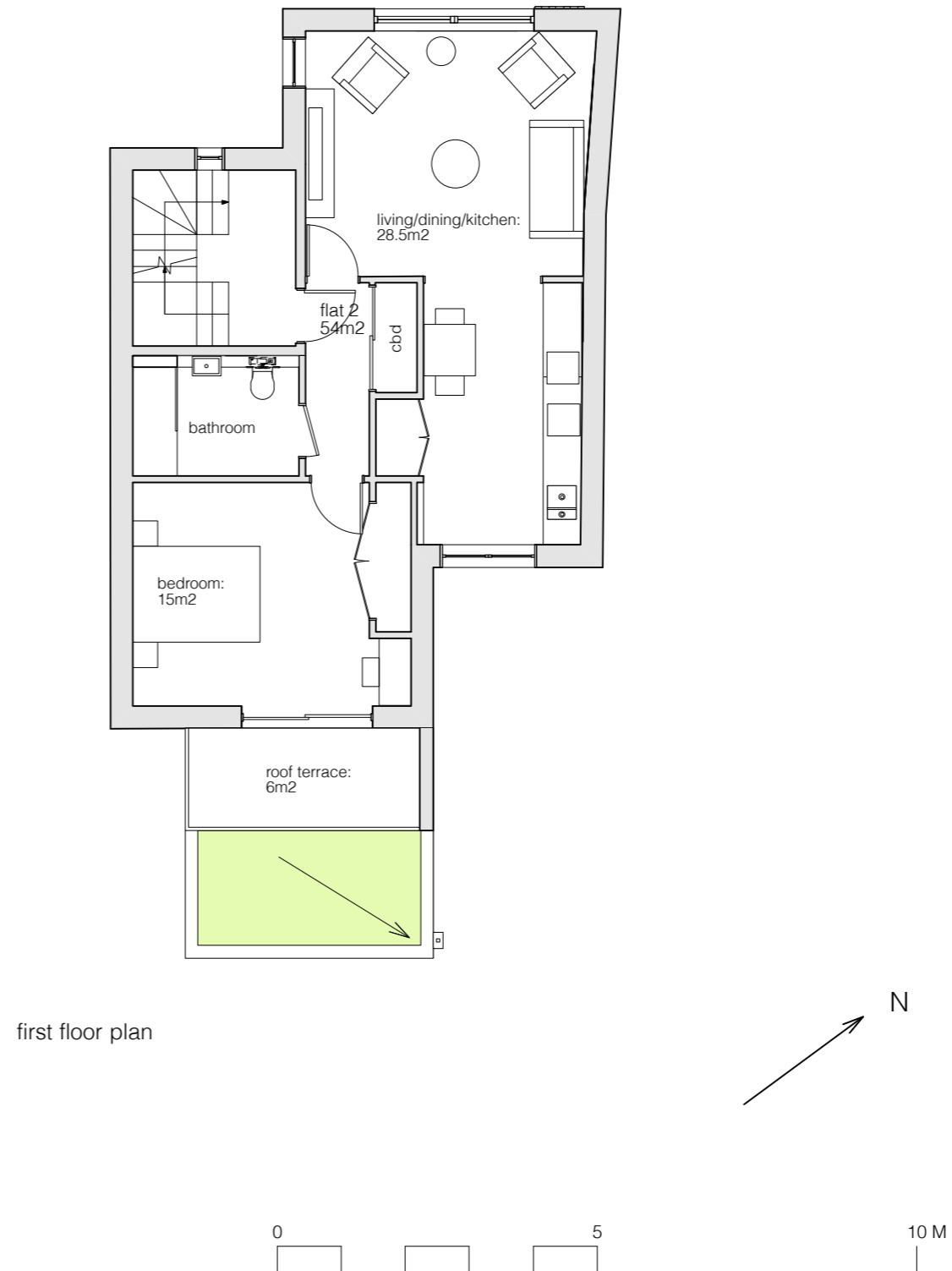
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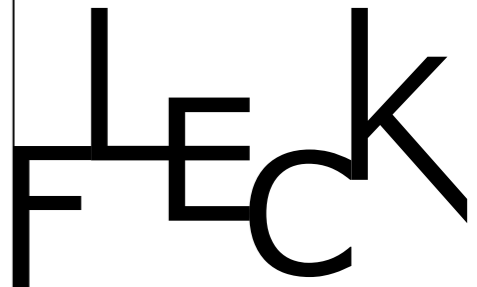
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CLIENT:
A. Azouz

PROJECT:
Proposed flats,
Pepys Road SW20

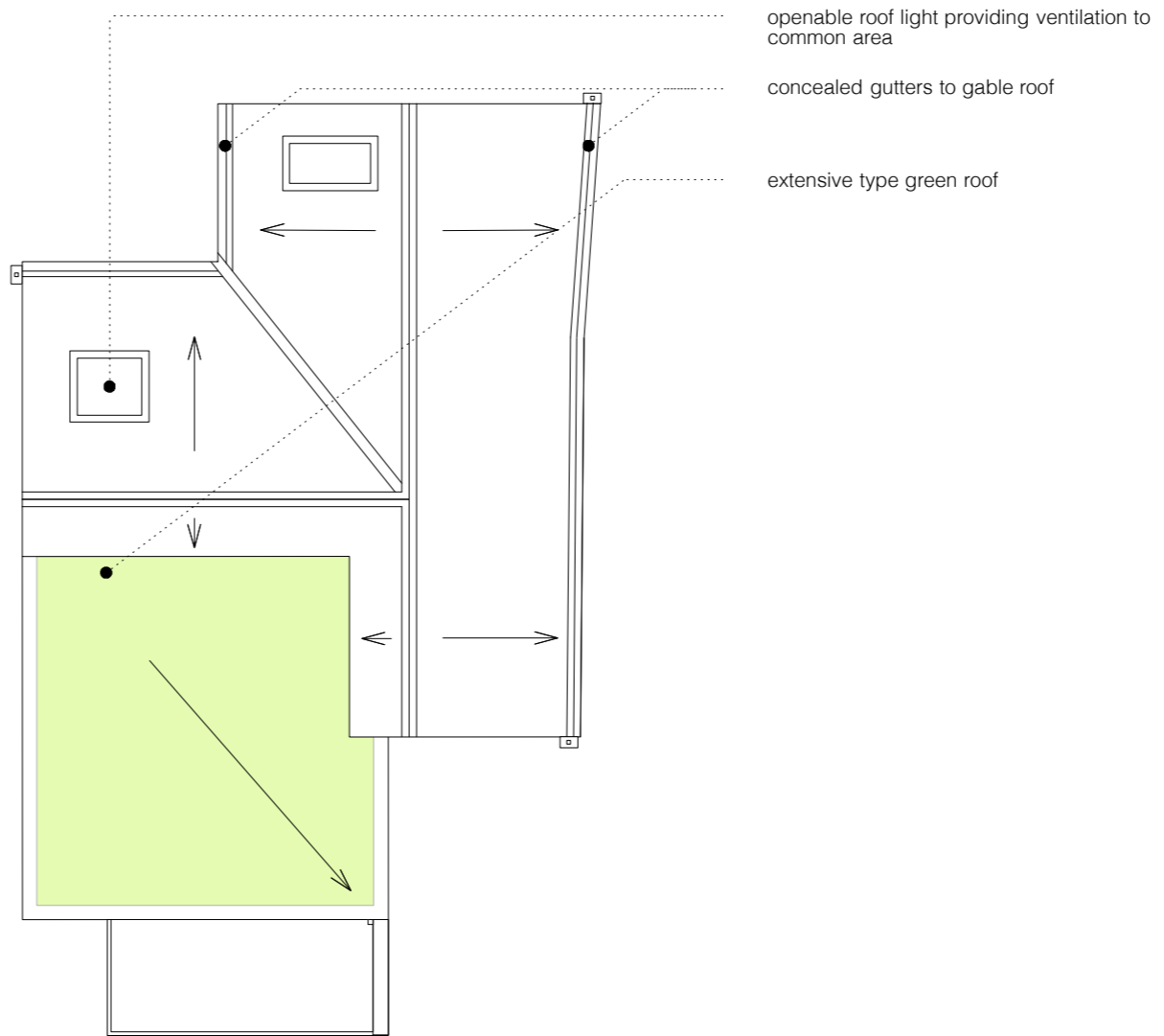
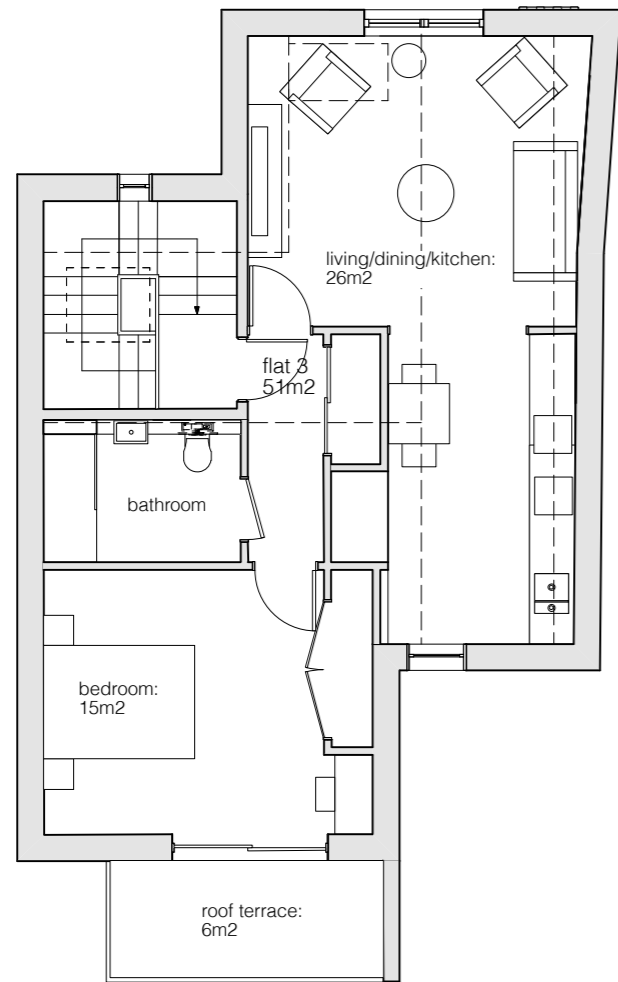
STATUS:
PLANNING

DRAWING:
Proposed ground and 1st floor plan

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DRAWING NUMBER: REV
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CLIENT:
 A. Azouz
 PROJECT:
 Proposed flats,
 Pepys Road SW20
 STATUS:
PLANNING

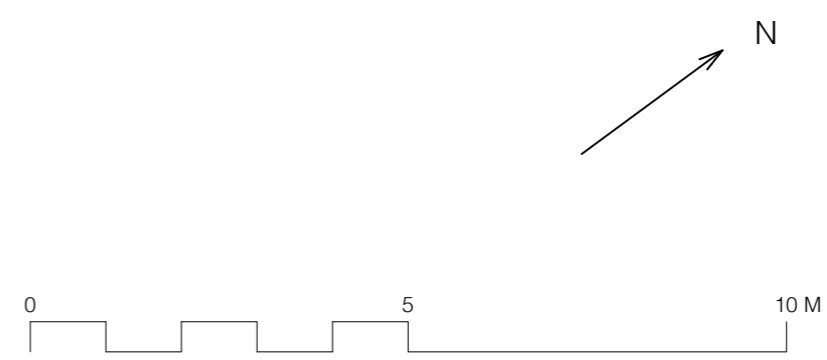
DRAWING:
 Proposed 2nd floor and roof plan

DRAWN BY: C F DATE: 0 8 0 9 2 1

SCALE: 1 : 1 0 0 @ A 3

DRAWING NUMBER: 1 1 5 8 P 2 0 1 REV

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284 Worple Road

Substation site

14 Pepys Road



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CLIENT:

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PROJECT:

Proposed flats,
Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed front elevation

DRAWN BY:

C F

DATE:

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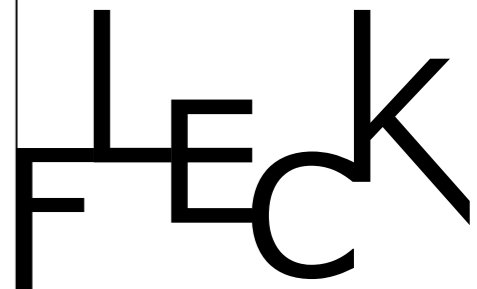




green wall formed of climbers rooted in soil

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Proposed flats,
Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed rear elevation

DRAWN BY: DATE:

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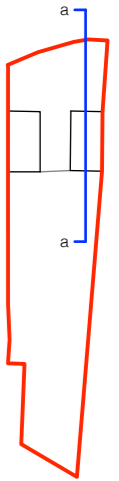




sectional elevation a-a

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 Proposed flats,
 Pepys Road SW20

STATUS:
PLANNING

DRAWING:
 Proposed sectional elevation

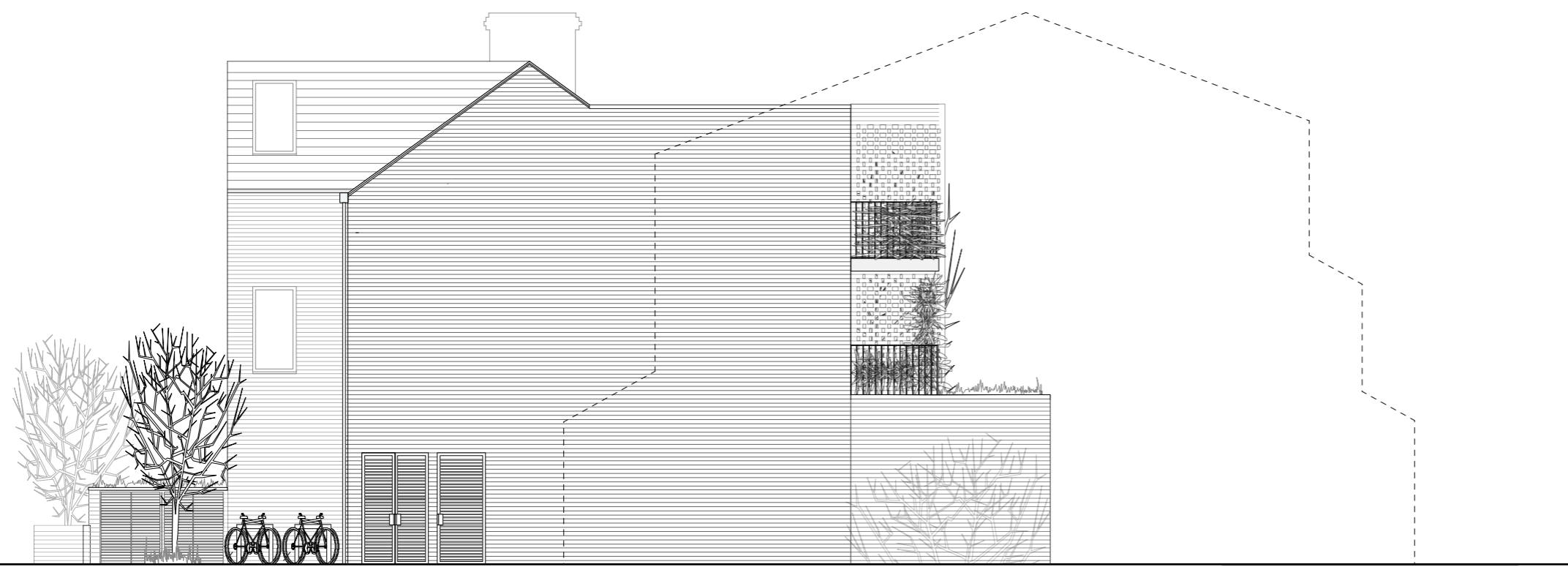
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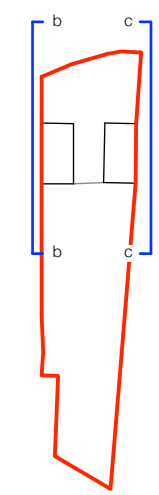
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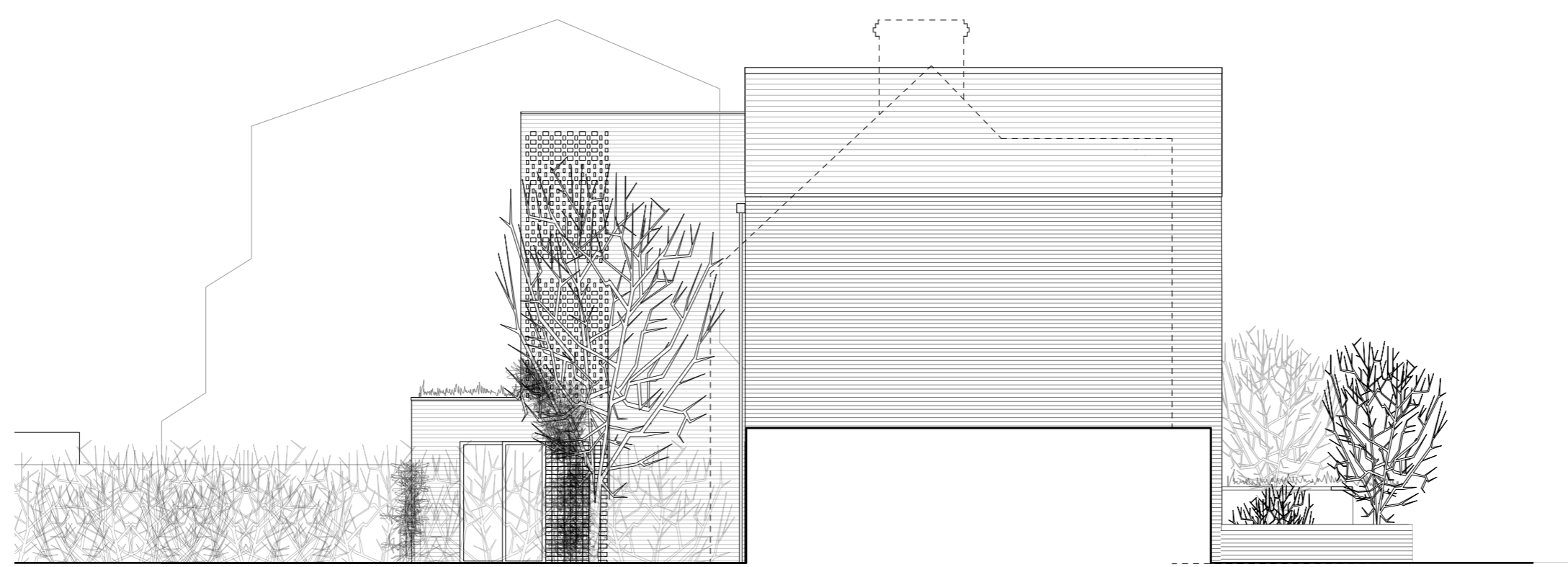
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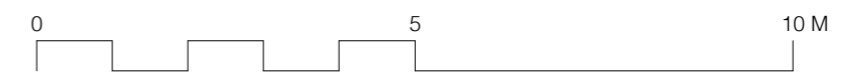
south west side elevation b-b



Page 118



north east side elevation c-c



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PROJECT:
 Proposed flats,
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STATUS:
PLANNING

DRAWING:
 Proposed side elevations

DRAWN BY: DATE:
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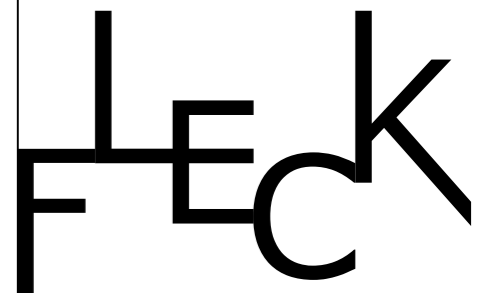
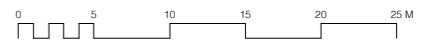
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N



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CLIENT: _____

A Azouz

PROJECT: _____

Pepys Road SW20

STATUS: _____

PRE-AP CONSULTATION

DRAWING: _____

SITE LOCATION PLAN

DRAWN BY: _____ DATE: _____

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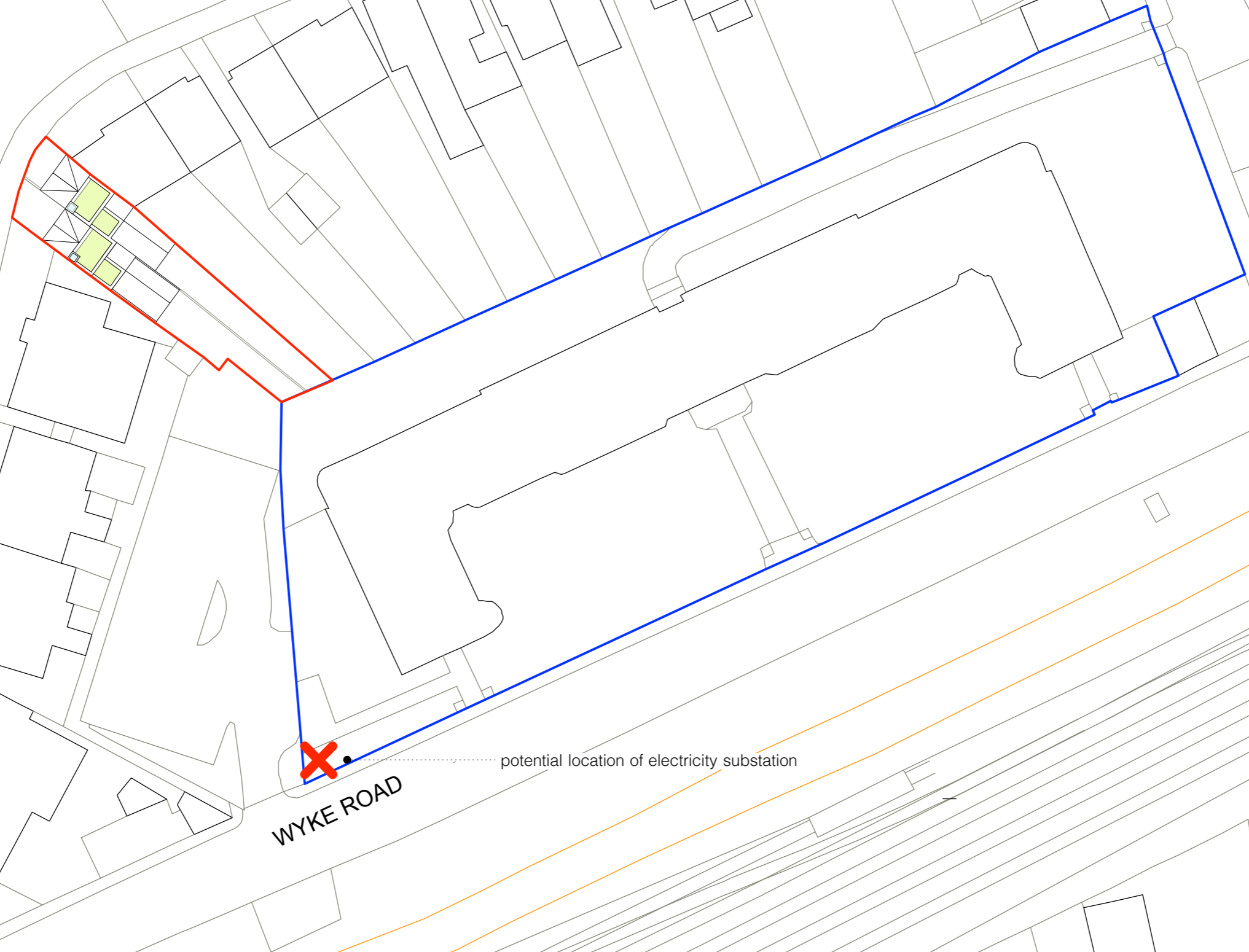
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PEPYS ROAD

WYKE ROAD

potential location of electricity substation



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PLANNING APPLICATIONS COMMITTEE
28th April 2022

<u>UPRN</u>	<u>APPLICATION NO.</u>	<u>Item No:</u> <u>DATE VALID</u>
	21/P3215	15/09/2021
Address/Site	2A Trinity Road, Wimbledon, SW19 8RL	
Ward	Trinity	
Proposal:	DEMOLITION OF EXISTING BUILDING AND ERECTION OF 18 X SELF-CONTAINED FLATS IN A PART THREE, PART FOUR STOREY, PART FIVE STOREY BUILDING AND ASSOCIATED WORKS	
Drawing Nos	See Condition 2	
Contact Officer:	Brenda Louisy-Johnson (0208 545 3169)	

RECOMMENDATION

Grant Planning Permission subject to conditions and completion of a S.106 Agreement.

CHECKLIST INFORMATION.

Heads of agreement: Car Park Permit Free, Zero Carbon contribution (£65,256), Affordable Housing (4 units and early and late stage viability review required).

Is a screening opinion required: No

Is an Environmental Statement required: No

Has an Environmental Impact Assessment been submitted – No

Press notice – Yes

Site notice – Yes

Design Review Panel consulted – No

Number of neighbours consulted – 410

External consultations –Thames Water

PTAL score – 6A/6B

Controlled Parking Zone (CPZ) – W3

1. **INTRODUCTION**

- 1.1 The application has been brought before the Planning Applications Committee for consideration as it falls outside of the Scheme of Delegation due to officer recommendation of grant permission subject to conditions and S106 agreement and the number and nature of objections received.

2. **SITE AND SURROUNDINGS**

- 2.1 The application site is 2A Trinity Road, the former Wimbledon Conservative Club. The former club is located on the west side of Trinity Road at its corner with South Park Road to the north. The former club is a part two storey, part single storey building. The two storey part of the building is a double height rendered bay under a flat roof and the single storey part is a brick built extension with a flat roof. The front of the site comprises tarmac and is used for vehicle parking. There is a single vehicle access taken directly from Trinity Road. Trinity Road runs north south connecting Queen's Road in the north with The Broadway in the south. The application site is located towards the southern end of Trinity Road next to the YMCA building which it shares its southern boundary with.
- 2.2 The townscape of the around the application site is mixed. The larger scale YMCA building to the south is 6-8 storeys, Keble Court to the north is a four storey purpose built block of flats, to the east is Nairn Court which comprises a series of three storey buildings of flats and to the west lies a purpose built blok of flats which rise to two stories under a pitched roof. The YMCA site which has planning permission for redevelopment extends to nine storeys in total and on the boundary with the application site extends to five storeys (currently under construction).

On the Merton Policies Map the site is located within the Town Centre Boundary, this is the only planning designation for the site.

3.0 **CURRENT PROPOSAL**

- 3.1 Planning permission is sought for the demolition of existing building and erection of 18 x self-contained flats in a part three, part four storey, part five storey building and associated works.
- 3.2 The massing of the proposed building is a direct response to the buildings surrounding the site. The five storey height of the building matches the lowest height of the stepped elevation of the approved YMCA building with its floor levels to match this building. The proposed building then steps down to also match the more domestic in scale residential buildings along South Park Road.
- 3.3 At the corner of Trinity Road and South Park Road the proposed building would built up this corner to match the other corners at the road junction where Keble Court and Nairn Court also residential blocks are present.
- 3.4 The amenity areas of the flats in the form of balconies face the public realm of Trinity Road and South Park Road. The roofs will allow for additional amenity space.
- 3.5 The materials pallet would be brick with zinc. The ground, first, second and third floors would have four flats each and the fourth floor would have two flats. The development would comprise 7 x 1 bedroom, 9 x 2 bedroom and 2 x 3 bedroom flats. Within the development all flats have access to amenity space in the form of either a balcony, small garden area or communal roof terrace.

4.0 PLANNING HISTORY

MER175/77 – Extension – Grant - 02/06/1977

MER766/74 - Use of 2nd floor as offices – Grant - 27/03/1975

MER958/73 - Escape staircase – Grant - 27/09/1973

MER48/73 - First floor wc extension – Grant - 22/03/1973

MER746/69 - Bar store – Grant - 11/09/1969

WIM6227 - Single storey extension – Grant - 01/05/1962

WIM5981 - Outline erection of a 4 storey building including 2 shops, entrance lounge, dining room, kitchen etc and a total of 102 hostel bedrooms and 2 three room flat on the 1st, 2nd and 3rd floors – Grant - 05/11/1961

5.0 CONSULTATIONS

- 5.1 The application has been advertised by Major site notice and press procedure, as well as 75 neighbours being consulted via letter. 35 representations have been received raising objection.

Reasons for objection:

- The height, size and massing is excessive
- The height will set a precedent
- Overdevelopment
- Overbearing
- The corner of The Broadway and Trinity Road will look like a fortress due to the YMCA building
- The development will add to the negative features of the YMCA
- The YMCA redevelopment should not set a precedent for this development
- It will change the character of Wimbledon
- Design is ugly, bland and uninteresting
- Out of keeping with the Victorian housing in the local area and the South Park Conservation Area
- Choice of materials is uninspiring, unimaginative, poor quality and will deteriorate in a few years
- There should be a dedicated large area for rubbish bins
- No clear landscaping proposals
- The building line does not match the neighbours in adjoining residential roads
- Pedestrian and traffic movements will increase
- It will put pressure on local parking which is already high
- The refuse lorries will have to sit a long time in the road to dispose of waste
- The site management team will move the bins to the kerb. Who is this mysterious management company?
- Delivery vehicles stopping on Trinity Road will cause mayhem
- The scheme needs to be redesigned to accommodate off-road servicing
- Do the owners of the development have legal right over the shared access with No.77 to access the bin store?
- Noise and dust pollution and vibration from construction work
- Noise pollution from the roof gardens
- Roof terraces should not be allowed due to overlooking
- The Sunlight and Daylight Report supports the view that the development is overdevelopment
- Loss of light to houses in South Park Road
- Reduction in sunlight and daylight will adversely affect the health of the vulnerable

- Rainwater harvesting should insisted upon
- The air-source heat pump and solar panels are added as a token after thought
- A condition is required prohibiting construction works at the weekend
- A condition is required prohibiting the balconies from being used to store bikes, prams and other items, it ruins the street view. There should be areas within the flats to store these items.
- There is a substantial difference between The Broadway and Trinity Road, the latter is residential. The description given by the developer regarding this is incorrect.
- The development is not sustainable in terms of schools, GPs and other facilities
- The site should be used for social meetings as originally intended
- Insufficient community engagement and support
- The Council should seek 40% affordable housing as Policy CS8 requires
- The developer wants to maximise the number of flats to make a profit

5.2 Friends of Wimbledon Town Centre:

- Excessive scale
- Lack of passive surveillance of streets
- Building lacks interest
- Flats 3 and 4 are single aspect
- Pressure to remove 3 trees near Trinity Road elevation

5.3 *Consultee Comments:*

5.4 Environmental Health Officer (Noise)

Conditions recommended relating to implementation of the recommendations in the submitted Noise Assessment Report and a post completion the submission of a noises assessment to ensure compliance and the other relating to the submission of a Construction Method Statement.

5.5 Environmental Health Officer (Air Quality)

3 conditions recommended,

5.6 Flood Risk Officer

Conditions recommended relating to submission of a detailed scheme for the provision of surface and foul water, a detailed design and specification for the green roofs and a detailed SuDS maintenance plan.

5.7 Highways Officer

Conditions recommended and 2 informatives.

5.8 Thames Water

Waste comments: with regard to waste water network and sewage treatment works infrastructure capacity, we would not have any objection to the planning application, based on the information provided. Water comments: with regard to water network and water treatment infrastructure capacity, we would not have any objection to the planning application. T.W recommend 1 condition relating to piling and 2 informatives.

5.9 Natural England

No comment.

5.10 Swift Conservation

As a minimum they recommend that the recommended ecological enhancements in the Eight Associates report are included in the planning conditions.

6.0 POLICY CONTEXT

6.1 National Planning Policy Framework 2021:

Chapter 1 Achieving sustainable development

Chapter 5 Delivering a sufficient supply of homes

Chapter 8 Promoting health and safe communities

Chapter 9 Promoting sustainable transport

Chapter 11 Making effective use of land

Chapter 12 Achieving well-designed places

Chapter 14 Meeting the challenge of climate change, flooding and coastal change

Chapter 15 Conserving and enhancing the natural environment

6.2 London Plan (2021) Policies:

SD6 Town centres and high streets

SD10 Strategic and local regeneration

D1 London's form, character and capacity for growth

D2 Infrastructure requirements for sustainable densities

D3 Optimising site capacity through the design-led approach

D4 Delivering good design

D5 Inclusive design

D6 Housing quality and standards

D7 Accessible housing
D8 Public realm
D11 Safety, security and resilience to emergency
D12 Fire safety
D13 Agent of Change
D14 Noise
H1 Increasing housing supply
H2 Small sites
H4 Delivering affordable housing
H5 Threshold approach to applications
H6 Affordable housing tenure
H7 Monitoring of affordable housing
H9 Ensuring the best use of stock
H10 Housing size mix
S4 Play and informal recreation
HC4 London View Management Framework
HC6 Supporting the night-time economy
HC7 Protecting public houses
G6 Biodiversity and access to nature
SI 1 Improving air quality
SI 2 Minimising greenhouse gas emissions
SI 3 Energy infrastructure
SI 4 Managing heat risk
SI 5 Water infrastructure
SI 7 Reducing waste and supporting the circular economy
SI 8 Waste capacity and net waste self-sufficiency
SI 12 Flood risk management
SI 13 Sustainable drainage
T1 Strategic approach to transport
T2 Healthy Streets
T3 Transport capacity, connectivity and safeguarding
T4 Assessing and mitigating transport impacts
T5 Cycling
T6 Car parking
T6.1 Residential parking
T7 Deliveries, servicing and construction
T9 Funding transport infrastructure through planning

6.3 Merton Local Development Framework Core Strategy (2011) Policies (Core Strategy)

CS8 Housing Choice
CS9 Housing Provision
CS11 Infrastructure
CS12 Economic Development
CS13 Open Space, Nature Conservation, Leisure and Culture
CS14 Design

CS15 Climate Change
CS16 Flood Risk Management
CS17 Waste Management
CS18 Active Transport
CS19 Public Transport
CS20 Parking, Servicing and Delivery

6.4 Merton Sites and Policies Plan (2014) Policies (SPP):

DM R5 Food and drink / leisure and entertainment uses
DM H2 Housing mix
DM H3 Support for affordable housing
DM C1 Community facilities
DM E4 Local employment opportunities
DM D2 Design considerations in all developments
DM D3 Alterations and extensions to existing buildings
DM D4 Managing heritage assets
DM EP1 Opportunities for decentralised energy networks
DM EP2 Reducing and mitigating noise
DM EP3 Allowable solutions
DM EP4 Pollutants
DM F1 Support for flood risk management
DM F2 Sustainable urban drainage systems (SuDS) and; Wastewater and Water Infrastructure
DM T1 Support for sustainable transport and active travel
DM T2 Transport impacts of development
DM T3 Car parking and servicing standards
DM T4 Transport infrastructure

6.5 Supplementary planning considerations:

London Housing SPG – 2016
Merton Design SPG – 2021
NPPG 2014
Affordable Housing and Viability SPG (2017) Mayor of London
Merton's Viability SPD 2018

7.0 PLANNING CONSIDERATIONS

- Principle of development
 - Demolition
 - Loss of Members Club
 - Residential
- Affordable Housing
- Housing Mix
- Standard of Accommodation
- Design
 - Massing, Scale, Height
 - Appearance, Siting, Layout

- Conservation Area
- Boundary Wall
- Cycle Storage
- Bin Storage
- Landscaping
- Neighbour Impact
- Sunlight and Daylight
- Noise
- The Broadway / Trinity Road YMCA
- 77 South Park Road
- Trinity Road
- Trees / Biodiversity
- Flood Risk / Drainage
- Transport, Servicing Delivery, Parking and Cycling
- Car Parking
- Cycle Parking
- Fire Safety
- Sustainability
- Air Quality

8.0 Principle of development

8.1 *Demolition*

There is no objection to demolition of the existing building as the site is not located within a conservation area and the building is not listed.

8.2 *Loss of Members Club*

The proposal would result in the loss of the existing private member's club. The existing use falls under a Sui-Generis use, which is a use which falls outside of the scheduled use classes. Its loss would therefore not result in the loss of any community use as set out in planning policy DM C1 (Community facilities) or any uses identified under planning policy DM R5 (Food and drink /Leisure and entertainment uses) of Merton's Sites and Policies Plan. In assessing the loss of the existing use, officers have taken a balanced view of its loss against the need to provide much needed new residential accommodation (as set out below), in a highly sustainable location. In this instance, the loss of the existing use is acceptable given the reasons stated above.

8.3 *Residential*

The National Planning Policy Framework 2019 and London Plan policies promote sustainable development that encourages the construction of additional dwellings at locations with good public transport accessibility. Core Strategy policies CS8 & CS9 seek to encourage proposals for well-designed and conveniently located new housing that will create socially

mixed and sustainable neighbourhoods through physical regeneration and effective use of space.

- 8.4 Planning Policy H1 (Increasing housing supply) of the adopted London Plan 2021 has significantly increased Merton's housing target over a 10 year period (2019/20 -2028/29) to 9,180 new homes. This equates to 918 homes annually, an increase of 507 compared to the former target (411) set out in Merton's current Sites and Policies Plan. The new target therefore seeks to deliver more than double the former annual target. This sets Merton a challenging target to deliver the expected number of new homes that London needs to meet demand.
- 8.5 The site has a site area of 0.06 ha. The site is therefore considered to fall under planning Policy H2 (Small Sites) of the London Plan 2021. Following on from the housing targets set out above, small sites are expected to deliver 2,610 new homes over the 10 year period (2019/20 - 2028/29). Policy H2 sets out that for London to deliver more of the housing it needs, small sites (below 0.25 hectares in size) must make a substantially greater contribution to new supply across the city. Therefore, increasing the rate of housing delivery from small sites is a strategic priority. Achieving this objective will require positive and proactive planning by boroughs both in terms of planning decisions and plan-making.
- 8.6 The borough's Core Planning Strategy states that that it is expected that the delivery of new residential accommodation in the borough will be achieved in various ways including development in 'sustainable brownfield locations' and "ensuring that it is used efficiently" (supporting text to Policy CS9). The application site is on brownfield land and is in a sustainable location adjacent to other existing residential properties.
- 8.7 In light of the above, considerable planning weight must therefore be given to the delivery of new homes as part of the assessment. The application site is located on a brownfield site within a location of excellent public transport infrastructure. The site is therefore considered to be in an ideal location for residential use that would promote sustainable development. The redevelopment of the site would bring forward 18 new residential units which will make a modest contribution to meeting housing targets and would provide a good mix of unit sizes that will assist in the delivery of a mixed and balanced community in a sustainable location. New housing is considered to be in accordance with the objectives of the NPPF, London Plan targets, and LBM policies.

9.0 Affordable Housing

- 9.1 The London Plan highlights delivering more genuinely affordable housing

- as a key issue for London. Meeting the need for around 43,500 homes per year, as established in the 2017 Strategic Market Housing Assessment will require an increase in affordable housing contributions from all sources. All schemes are expected to maximize the delivery of affordable housing and make the most efficient use of all available resources. This is critical to enabling London help meet the needs of its workforce and maintain the function and resilience of the city.
- 9.2 The Council's policy on affordable housing is set out in the Core Planning Strategy, Policy CS8. For schemes providing over ten units, the affordable housing target is 40% (of which 60% should be social rented and 40% intermediate), which should be provided on-site.
- 9.3 In seeking this affordable housing provision, officers will have regard to site characteristics such as site size, site suitability and economics of provision such as financial viability issues and other planning contributions.
- 9.4 The Mayor's SPG on affordable housing and viability (Homes for Londoners) 2017 sets out that: "Applications that meet or exceed 35 per cent affordable housing provision (by habitable rooms) without public subsidy, provide affordable housing on-site, meet the specified tenure mix, and meet other planning requirements and obligations to the satisfaction of the LPA and the Mayor where relevant, are not required to submit viability information. Such schemes will be subject to an early viability review, but this is only triggered if an agreed level of progress is not made within two years of planning permission being granted (or a timeframe agreed by the LPA and set out within the S106 agreement)...
... Schemes which do not meet the 35 per cent affordable housing threshold, or require public subsidy to do so, will be required to submit detailed viability information (in the form set out in Part three) which will be scrutinised by the Local Planning Authority (LPA)."
- 9.5 If the proposal does not meet this 35% provision, it will be necessary to submit a full viability assessment in order to demonstrate that the scheme is delivering as much affordable housing as is financially viable.
- 9.6 The Applicant has submitted a Viability Report which concludes that the scheme would not be economically viable were there to be affordable housing provision on site and can only provide an off-site contribution of £205,292. However, the independent viability assessment by the Council's Consultants reveals that they consider the surplus to be £840,262, which equates to to 3 x social rent and 1 x shared ownership homes on site equating to 22% affordable housing on site provision. Therefore the Council's Consultants considers that the scheme can deliver 4 on-site affordable housing units.

9.7 The independent viability assessors recommend that the council applies the viability review mechanisms at early and late stages of development as outlined within the London Plan and Mayors SPG.

10.0 Housing Mix

10.1 Planning policy DM D2 (Housing Mix) of Merton's Sites and Polices Plan seeks to create socially mixed communities, catering for all sectors of the community by providing a choice of housing with respect to dwelling size and type in the borough. The borough level indicative proportions concerning housing mix (as set out below) will be applied having regard to relevant factors including individual site circumstances, site location, identified local needs, economics of provision such as financial viability and other planning contributions.

10.2 Table in Planning policy DM H2 (Housing Mix) of Merton's Sites and policies plan 2014

Number of bedrooms	Percentage of Units
One	33%
Two	32%
Three +	35%

10.3 The proposed development would deliver 7 x 1 bedroom flats, 9 x 2 bedroom flats and 2 x 3 bedroom flats. The proposed development would therefore generate a percentage housing mix as follows:

Number of bedrooms	Percentage Units
One	39%
Two	50%
Three +	11%

10.4 The proposal does not strictly meet the housing mix requirements, however the Borough level is indicative having regard to the site circumstances, site location and economic provision such as financial viability. The proposal is considered to offer a reasonable range of unit sizes, including 60% of family sized accommodation (2+ bedroom units). The site is in a highly sustainable location immediately adjacent to the town centre of Wimbledon wherein increase density of smaller flats is expected. The proposed mix is therefore considered to be acceptable.

11.0 Standard of accommodation

11.1 Core Planning Policy CS 14 and SPP policies DM D1 and DM D2 seek to

- ensure that new residential development is of a high standard of design both internally and externally and provides accommodation capable of adaptation for an ageing population and for those with disabilities, whilst offering a mix of unit size reflective of local need.
- 11.2 Planning Policy D6 (Housing quality and standards) of the adopted London Plan 2021 states that housing development should be of high quality design and provide adequately sized rooms with comfortable and functional layouts which are fit for purpose and meet the needs of Londoners without differentiating between tenures. The design of development should provide sufficient daylight and sunlight for future occupiers, have adequate and easily accessible storage space and maximise the provision of dual aspect dwellings (normally avoiding the provision of single aspect dwellings).
- 11.3 Dual Aspect
Dual aspect units have many inherent benefits, including better daylight, a greater chance of direct sunlight for longer periods, cross ventilation, a choice of views, access to a quiet side of the building, and greater flexibility in the use of rooms. A dual aspect dwelling is one with opening windows on two external walls, which may be on opposite sides of the building or around a corner. One aspect may be towards an external access deck or courtyard, although the layout of the dwelling needs to be carefully considered in these cases to maintain privacy.
- 11.4 The proposed development includes a high number of dual aspect flats. Some of the flats do include obscured glazing for side windows which doesn't totally fulfil the benefits of a dual aspect unit, however the windows can offer some natural ventilation if the upper part of the windows is openable. These windows have been designed to allow opening at the upper level (1.7m above internal floor level).
- 11.5 Planning Policy D6 (Housing quality and standards) seeks to maximise the provision of dual aspect dwellings and although not all flats are dual aspect, the development the layout of the flats are considered acceptable.
- 11.6 Sunlight and Daylight & Outlook
- 11.7 The development has been designed so that all habitable rooms would have good levels of outlook and light provision. The large window/door openings serving each of the main living spaces of the flats would ensure a high quality environment for future occupiers.
- 11.8 Space Standards
- 11.9 The proposal demonstrates that each flat would meet minimum Gross

Internal Area (GIA) standards as set out in Planning Policy D6 (Housing quality and standards). The space standards in this policy also require single bedrooms to be 7.5sqm and double bedrooms to be 11.5sqm. The proposal satisfies these requirements.

11.10 Amenity Space

Planning Policy D6 (Housing quality and standards) of the London Plans 2021 states that a minimum of 5 sqm of private outdoor space should be provided for 1-2 person dwellings and an extra 1 sqm should be provided for each additional occupant, and it must achieve a minimum depth and width of 1.5m. The proposal meets these requirements with the exception of the one bedroom flats numbered 8, 12, 16 and 18 which have access to a 65sqm 4th floor communal roof terrace. These flats have deliberately been designed without balconies to prevent overlooking into the rear garden areas of the houses along South Park Road. It is also noted that South Park Gardens is also within a short walking distance of the site which provides ample good quality open space for future occupiers. The total amenity space for the 18 flats is 198sqm (11sqm per flat) which is considered acceptable. The communal amenity space would have a mixture of planting and seating with a small children's play area / sandpit.

11.11 Playspace

Planning policy S4 (Play and informal recreation) of the adopted London Plan 2021 state that development proposals for schemes that are likely to be used by children and young people should:

- 1) increase opportunities for play and informal recreation and enable children and young people to be independently mobile
- 2) for residential developments, incorporate good-quality, accessible play provision for all ages. At least 10 square metres of playspace should be provided per child that:
 - a) provides a stimulating environment
 - b) can be accessed safely from the street by children and young people independently
 - c) forms an integral part of the surrounding neighbourhood
 - d) incorporates trees and/or other forms of greenery
 - e) is overlooked to enable passive surveillance
 - f) is not segregated by tenure

Given the modest size of the development and constraints of the site, providing on-site play space is demanding without reducing the amount of development. Officers do not consider that the quantum of development should be reduced to accommodate playspace (if the amount of playspace below cannot be delivered). The scheme as designed, benefits from a 73 sqm communal fourth floor roof terrace. Based on the GLA playspace calculator, the scheme, if market and intermediate units, the playspace requirement for this development would be 33.5sqm (18 flats). While the communal amenity space on the fourth floor roof terrace is not exclusively

for children, due to the constraints of the site it provides a dedicated area for children. As such the play space provision is considered acceptable in this case.

12.0 Design and visual amenity

12.1 Planning policy DM D2 (Design considerations in all developments) of Merton's Sites and Policies Plan seeks to achieve high quality design and protection of amenity within the Borough. Proposals for all development will be expected to relate positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings and existing street patterns, historic context, urban layout and landscape features of the surrounding area.

12.2 Massing, scale and height

Officers consider that the proposal makes affective use of the land by positively and appropriately relating to the siting, rhythm, scale, density, proportions, height, materials and massing of surrounding buildings and existing street patterns, historic context, urban layout and landscape features of the surrounding area. The proposal would provide a strong relationship with the recently approved YMCA development, the 3 storey building (with accommodation within the roof) on the opposite corner (1-9 Keble Court) and lower forms of development adjacent at 77 South Park Road and beyond. The massing, scale and height has also been broken down by splitting the building into two elements (stepping down the development into a part 3, part 4 and part 5 storey building and including balconies mainly on the north facing elevation) and with the clever use of materials such as large sections of glazing.

12.3 Appearance, siting and layout

There is not a dominant material for the buildings in the surrounding area. Stock and terracotta brick, timber and white render can be found in buildings. The appearance of the proposed buildings appears to be of high quality with materials comprising brick, zinc and significant glazing. The choice of materials for the proposed building will add to the existing variety. The layout of rooms with dual aspect maximises light and solar gain given the constraints of the site. The siting of the building right up to the corner of the plot builds up the corner plot to match the other corners at the road junction of Trinity Road and South Park Road. The siting of the building up to the flank wall of the YMCA site under construction compliments the urban form. It is considered that the appearance, siting and layout of the development are acceptable.

12.4 Conservation Area

The South Park Garden Conservation Area boundary is located one road to the north of the application site. As set out above, the proposed

development is considered to respect the context of the site and street scenes with the use of high quality materials and varied building heights, stepping down as it extends north away from the town centre. The combination of this and the distance to the Conservation Area boundary, satisfied officers that the proposal would not harm its setting. The proposed development is therefore considered to preserve the character and appearance of the South Park Garden Conservation Area and would not cause any harm to its setting.

12.5 Boundary Wall

The existing boundary fencing is considered to be a negative element within the street scene. The proposal includes a new low level boundary wall with a hedge behind and mature trees. The new boundary wall has been designed to respect the proposed building with the use of the same materials).

12.6 Cycle Storage

Cycle storage has been provided for flats 1 and 2 and visitor cycle storage (4 cycle parking spaces) to the side of flat 2 has been integrated in the boundary wall so it is concealed and is not visible from the public realm. Communal cycle storage (28 cycle parking spaces comprising double stacked cycle parking) can be found in the south west corner of the site adjacent the bin store and is accessed via the side access road between the application site and 77 South Park Road. All cycle storage is secure and covered.

12.7 Bin Storage

Bin storage is at the rear of the site and would be accessed in the same way as the communal cycle store in the south west corner of the application site. The side access road from where the bins can be accessed would be used for refuse collection. The bin store is within the required 20m of the public highway, an acceptable distance for the Council's refuse collection. Proposed are 5 bins with a total capacity of 5500 litres.

12.8 Landscaping

There is limited scope for soft landscaping within the site given its constraints, however, given the constraints a good level of soft landscaping at ground floor level has been included (comprising grass, hedges and trees along the north and east boundaries of the site) with limited hard standing (a negative element of the existing site). Private ground floor gardens provide a good buffer between the proposed building and the public pavement. Appropriate soft landscaping can be secured via condition.

13.0 Neighbour Amenity

- 13.1 SPP Policy DM D2 states that proposals must be designed to ensure that they would not have an undue negative impact upon the amenity of neighbouring properties in terms of natural light (sunlight and daylight), outlook, privacy, noise, visual intrusion and quality of living conditions.
- 13.2 Sunlight and Daylight
The Applicants have submitted a daylight and sunlight report assessing the impact of the proposed building on neighbouring properties. The Planning Statement summarises the findings of the sunlight and daylight assessment:
*“In terms of daylight, all neighbouring buildings were assessed. All windows past the BRE test except that is six (three of which are bedrooms and the remaining three are secondary windows) out of fifteen in the neighbouring YMCA scheme as well as three windows in no.77 South Park Road. The relevant windows which would be affected are not the main windows to the rooms which remain unaffected. Out of 113 windows assessed 104 (or 92%) will continue to meet the target values set out in the BRE guide.
In terms of daylight distribution, 63 rooms were tested and 54 (86%) passed. Out of the rooms which did not meet the BRE guidance, these were either bedrooms (in the case of 196-200 The Broadway) where the effective enjoyment would not rely on good levels of natural daylight amenity. In the case of Nairn Court and 77 South Park Road, the affected rooms would still receive relatively good levels of daylight in line with those typically achieved in urban areas.
Results of 80 out of 82 windows assessed for sunlight met with the BRE Guidance. The two windows that did not meet the relevant test would fall 5% short of the target during the winter months but still far exceeds the annual sunlight hours.
In respect of overshadowing, there would be a small reduction in the overshadowing to the external areas of 77 Home Park Road with 4.05% of the area receiving 2 hrs of direct sunlight”*
- 13.3 The report concludes that proposed development would not cause material impacts on the daylight and sunlight amenity of the neighbouring buildings and their external amenity areas. The technical analysis demonstrates that the majority of rooms and windows will meet the numerical criteria for daylight and sunlight amenity to surrounding properties.
- 13.4 Based on these findings it is considered by officers that the proposed development would not cause material harmful impacts on the daylight and sunlight amenity of the neighbouring buildings and their external amenity area. The technical analysis demonstrates that the majority of

rooms and windows would meet the numerical criteria for daylight and sunlight amenity to surrounding properties and where rooms and windows do not meet the numerical criteria for daylight and sunlight amenity the windows are not windows to habitable rooms or the rooms and windows are not significantly adversely affected.

13.5 Noise

The Applicant has submitted a noise report which assesses the impact of external noise on habitable rooms and outdoor amenity areas and also looks at the impact of noise to habitable rooms from ventilation. The report concludes that with sufficient mitigation measures such as trickle ventilation, appropriate wall cavity insulation and a combination of brick walls and close boarded timber fencing to a specified height internal and external noise could be kept to a minimum.

13.6 The following assessment of impact on amenity looks at outlook and privacy.

13.7 The Broadway/Trinity Road YMCA

The adjoining site to the south currently comprises a non-residential use. Therefore there would be no undue loss of amenity to this neighbouring property. However, the proposal also needs to be assessed against the recent YMCA planning permission which would include residential units adjacent to the application site. The proposed development is considered to respect the layout and form of the recently approved YMCA scheme. In particular, the proposed building would respond to the courtyard design approach (open on the side with the application site). The courtyard design of the YMCA building would provide outlook and light to some of the new YMCA flats. The proposal would still ensure that outlook from these neighbouring windows is maintained to a reasonable level. There is obscure glazing to the windows in the proposed development facing the courtyard windows in the YMCA building to prevent overlooking and loss of privacy.

13.8 77 South Park Road

The adjoining site to the west comprises a two storey block of 4 flats. A side access road sits between the application site and this neighbouring block of flats provides a good level of separation which will help to mitigate impact on these neighbouring properties.

13.9 The flank wall facing the application site does include a number of windows, however, these windows would not be overlooked because the facing windows in the proposed development would be obscure glazed to a height of 1.7m and there are no balconies facing in the proposed development facing this flank wall.

13.10 The proposed building would project beyond the front and rear elevations of this neighbouring block of flats, however given the level of separation between neighbours it is considered that there would be no undue loss of amenity in terms of outlook.

13.11 Residential properties on east side of Trinity Road and north side of South Park Road

The neighbouring residential buildings to the north and east of the application site are located on the opposite sides of these highways. A good level of separation therefore would exist to provide breathing space from the proposal. The proposed building is not considered to be overly bulky to cause harm to the outlook from these neighboring properties. The separations distances between the proposal and these neighboring residential properties is suitable given the town centre location.

14.0 Trees / Biodiversity

14.1 Planning Policy DM O2 (Nature conservation, trees, hedges and landscape features) of Merton's Sites and Policies Plan seeks to protect and enhance biodiversity.

14.2 The pre-application site is not located within a conservation area and no trees on the site are protected by tree preservation orders. There are no trees on the site which would be affected by the proposal.

14.2 The Applicant has submitted a Preliminary Ecological Appraisal. A desk study and Phase 1 habitat survey were undertaken to review the ecology. The site was found to have low ecological value, providing some habitat for nesting birds. The development is expected to have little impact on designated statutory sites near to the development. The Appraisal makes recommendations to mitigate any impact from the development. It is considered that if all recommendations within this report are implemented, it is thought that the development will have minimal impact on the ecology of the site and zone of influence. There would be a net gain in biodiversity due the inclusion of soft landscaping (the existing site has no soft landscaping) and three green roofs. It is considered that the proposal would comply with the above policy.

15.0 Flood Risk / Drainage

15.1 Planning Policy SI 12 (Flood risk management) of the adopted London Plan 2021 states that development proposals should ensure that flood is minimised and mitigated, and that residual risk is addressed. Planning Policy SI 13 (Sustainable drainage) of the London Plan 2021 states that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as

possible.

- 15.2 Merton's Core Planning Policy CS16 and SPP Policies DM F1, DM F2 and DM D2 all seek to ensure that adequate flood risk reduction measures, mitigation and emergency planning are in place to ensure there is no increase in flood risk off-site or to the proposed development.
- 15.3 The application site is located in flood zone 1 (low risk). It is proposed to provide a new connection to the surface water sewer in South Park Road. It is proposed to reduce the peak surface water run-off rates as far as it is reasonably practicable. All new hardstanding would be permeable or designed to shed onto soft areas. Three green roofs are proposed in the development. The proposals provide significant betterment to the peak surface water run-off rate to that of the existing site.
- 15.4 The Council's Flood Risk Officer has also reviewed the sustainable drainage strategy and has no objection and recommends three conditions relating to submission of details of a scheme for the provision of surface and foul water drainage, design and specification for the green roofs and a SuDS maintenance plan.

16.0 Transport, Servicing Delivery, Parking and Cycling

- 16.1 Policy T1 (Strategy Approach to Transport) of the London Plan 2021 states that the Mayor has a strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041. All development should make the most effective use of land, reflecting its connectivity and accessibility by existing and future public transport, walking and cycling routes and to ensure that any impacts on London's transport networks and supporting infrastructure are mitigated.
- 16.2 At a local level Policy CS19 of Merton's Core Planning Strategy states that the Council will ensure that all major development demonstrates the public transport impact through transport assessments. Travel plans will also be required to accompany all major development. Policy CS18 promotes active transport and encourages design that provides attractive safe, covered cycle storage, cycle parking and other facilities (such as showers, bike cages and lockers).
- 16.3 Car Parking
- 16.4 Policy CS18 (Car Parking) of the London Plan 2021 states that car-free development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking. Car-free development has no general parking but should provide

disabled person's parking in line with Part E of this policy.

16.5 The proposal would result in the removal of existing car parking spaces on the site and no on-site car parking being proposed. The development would therefore be a car-free development. The application site is located within an existing controlled parking zone, W3, which is already overly subscribed and has excellent access to public transport (PTAL score 6a). The proposed development is considered to be suitable as a car-free and permit free development in accordance with planning policy T6. As part of this full planning application the permit free development would need to be secured in a section 106 agreement which would restrict future occupiers of the development from obtaining an on-street residential parking permit to park in the surrounding controlled parking zones.

16.6 Cycle Parking

16.7 Planning Policy T5 (Cycling) of the London Plan 2021 states that development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle.

16.8 Policy T5 requires 1.5 spaces per 1 bedroom flat and 2 spaces per flat for larger flats with visitor parking for every 40 units. The proposal consists of 18 dwellings with 7 x 1 bedroom, 9 x 2 bedroom and 2 x 3 bedroom flats, therefore the total cycle parking would equate to 34 spaces throughout the proposed development (including one visitor space). The proposal shows a total of 36 cycle spaces which includes 32 long-term spaces and 4 short-term stay spaces (visitor spaces). Therefore, the proposal would comply with the above policy with respect to cycle parking.

17.0 Fire Safety

17.1 Planning Policy D12 (Fire Safety) of the London Plan 2021 highlights that the fire safety of developments should be considered from the outset. How a building will function in terms of fire, emergency evacuation, and the safety of all users should be considered at the earliest possible stage to ensure the most successful outcomes are achieved, creating developments that are safe and that Londoners can have confidence living in and using.

17.2 The Applicant has complied with the above policy and submitted a fire strategy. A suitable condition can be imposed to ensure that the development meets fire regulations once completed.

18.0 Sustainability

18.1 Planning Policy SI 2 (Minimising greenhouse gas emissions) of the

London Plan 2021 states that all major development should be net zero-carbon. This means reducing greenhouse gas emissions in operation and minimising both annual and peak energy demand in accordance with the following energy hierarchy:

- 1) be lean – use less energy and manage demand during operation
- 2) be clean – exploit local energy resources (such as secondary heat) and supply energy efficiently and cleanly
- 3) be green – maximise opportunities for renewable energy by producing, storing and using renewable energy on-site
- 4) be seen – monitor, verify and report on energy performance

Major developments should demonstrate in a detailed energy strategy how the zero-carbon target will be met within the framework of the energy hierarchy.

18.2 A minimum on-site reduction of at least 35 per cent beyond Building Regulations is required for major development. Residential development should achieve 10 per cent and non-residential should achieve 15 per cent through energy efficiency measures. Where it is clearly demonstrated that the zero-carbon target cannot be fully achieved on-site, any shortfall should be provided in agreement with the borough either:

- 1) through a cash in lieu contribution to the borough's carbon offset fund, or
- 2) off-site provided that an alternative proposal is identified and delivery is certain.

18.3 Major development proposals should calculate and minimise carbon emissions from other part of the development including plant or equipment that are not covered by Building Regulations i.e. unregulated emissions.

18.4 The Applicant has submitted an energy strategy which demonstrates how the proposed development will aim to meet the policy requirement of achieving 100% reduction in carbon emissions through the be lean, be clean, be green hierarchy.

18.5 Improvements will be made to the fabric and plant proposed for the development. The measures proposed are summarised as follows:

- Communal air source heat pump
- Significant fabric improvement
- Good air tightness
- Accredited Construction Details
- 12.6Kwp of PV panels

18.6 These measures will result in a 72% improvement being met. This means that through the reduction in carbon emissions from the be lean to the be green stages that the London Plan requirement for a 10% reduction is met at the be lean stage. The total remaining regulated emissions are 5 tonnes and the total unregulated emissions are 17.9 tonnes and the offset payment requires £95 per tonne over 30 years. Therefore, the total offset payment is $(22.9 \times 95 \times 30) = £65,256$. This monies would be secured via the S106 Agreement.

19.0 Air Quality

19.1 Planning Policy SI 1 (Improving air quality) of the London Plan 2021 seeks to tackle poor air quality and protect health.

19.2 The Applicant has submitted an Air Quality Assessment. There are potential impacts to air quality affecting amenity and human health from construction activities, demolition, earthworks and construction traffic. The assessment concludes that with appropriate construction phase mitigation air quality can be satisfactory management. Officers are in agreement with this conclusion and the Environmental Health Officer recommends 3 conditions relating to the submission of a Demolition and Construction Management Plan to be written in accordance with Transport for London Construction Logistics Plan guidance and London Borough of Merton SPD 'Air Quality', and the development not being implemented other than following the approved scheme. As such, it is considered that the proposal would comply with the above policy.

20.0 CIL

20.1 The proposed development would be subject to the Community Infrastructure Levy (CIL). This would require a contribution of £306 per additional square metre of floor space to be paid to Merton Council and an additional £60 per additional square meter to be paid to the Mayor. Further information on this can be found at:

<http://www.merton.gov.uk/environment/planning/cil.htm>

CONCLUSION

There is no objection to the loss of the existing building and its use. The proposal would provide new housing in a highly sustainable location, making good use of a small brownfield site. The overall scale, height and relationship to surrounding neighboring properties is acceptable and would not cause material harm to neighboring amenity. It is considered that the proposal is acceptable with respect to all planning considerations and would comply with all relevant planning

policies. As such it is recommended that planning permission be granted, subject to conditions and S106 Agreement.

RECOMMENDATION

Grant planning permission subject to conditions and S106 Agreement

- 1 The development to which this permission relates shall be commenced not later than the expiration of 3 years from the date of this permission.

Reason: To comply with Section 91 (as amended) of the Town & Country Planning Act 1990.

- 2 The development hereby permitted shall be carried out in accordance with the following approved plans: 241(10) 001 Rev A; (10) 002 Rev A; (10) 003 Rev A; 241 (11) 001 Rev A; (11) 001 Rev A Section AA; (11) 002 Rev A; (11) 003 Rev A; Accommodation Schedule; Air Quality Assessment; Daylight and Sunlight Report; Design & Access Statement; Energy Statement; Fire Strategy; Noise Report; Phase 01 Report Part 01 & Part 02; Planning Statement; Preliminary Ecological Appraisal; Statement of Community Involvement; SuDS Report; & Transport Statement.

Reason: For the avoidance of doubt and in the interests of proper planning.

- 3 The facing materials to be used for the development hereby permitted shall be those specified in the application form unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for Merton: policies D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

- 4 No development shall take place until details of the surfacing of all those parts of the site not covered by buildings or soft landscaping, including any parking, service areas or roads, footpaths, hard and soft have been submitted in writing for approval by the Local Planning Authority. No works that are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details have been approved and works to which this condition relates have been carried out in accordance with the approved details.

Reason: To ensure a satisfactory standard of development in accordance

with the following Development Plan policies for Merton: policy D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D1 and D2 of Merton's Sites and Policies Plan 2014.

- 5 No development shall take place until details of all boundary walls or fences are submitted in writing for approval to the Local Planning Authority. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details are approved and works to which this condition relates have been carried out in accordance with the approved details. The walls and fencing shall be permanently retained thereafter.

Reason: To ensure a satisfactory and safe development in accordance with the following Development Plan policies for Merton: policies D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D1 and D2 of Merton's Sites and Policies Plan 2014.

- 6 No development shall take place until details of the proposed finished floor levels of the development, together with existing and proposed site levels, have been submitted to and approved in writing by the Local Planning Authority, and no development shall be carried out except in strict accordance with the approved levels and details.

Reason: To safeguard the visual amenities of the area and to comply with the following Development Plan policies for Merton: policy D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

- 7 Due to any potential impact of the surrounding locality on the development the recommendations to protect noise intrusion into the residential dwellings as specified in the Cole Jarman, Noise Assessment Report Ref:206/0074/R1, dated July 2021 shall be implemented as a minimum standard. A post completion noise assessment to ensure compliance shall be undertaken and submitted to the LPA.

Reason: To protect the amenities of the occupiers in the adjoining residential premises and future occupants and to comply with Merton's Site and Policies Plan 2014 Policy DM D2.

- 8 No development shall take place until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the demolition and construction period.

The Statement shall provide for:

- hours of operation
- the parking of vehicles of site operatives and visitors
- loading and unloading of plant and materials
- storage of plant and materials used in constructing the development
- the erection and maintenance of security hoarding including decorative - displays and facilities for public viewing, where appropriate
- wheel washing facilities
- measures to control the emission of noise and vibration during construction/demolition.
- demonstration to show compliance with BS5228
- measures to control the emission of dust and dirt during construction/demolition
- a scheme for recycling/disposing of waste resulting from demolition and construction works

Reason: To protect the amenities of the occupiers in the adjoining residential premises and future occupants.

- 9 1. Prior to the commencement of development, including demolition, a Demolition and Construction Environmental Management Plan (DCEMP) shall be submitted to and approved in writing by the Local Planning Authority. The DCEMP shall include:
- a) An Air quality management plan that identifies the steps and procedures that will be implemented to minimise the creation and impact of dust and other air emissions resulting from the site preparation, demolition, and groundwork and construction phases of the development.
 - b) Construction environmental management plan that identifies the steps and procedures that will be implemented to minimise the creation and impact of noise, vibration, dust and other air emissions resulting from the site preparation, demolition, and groundwork and construction phases of the development.
 - c) Construction Logistics Plan that identifies the steps that will be taken to minimize the impacts of deliveries and waste transport.
2. The above plans shall not be written other than in accordance with TfL (Transport for London) Construction Logistics Plan Guidance and London Borough of Merton SPD 'Air Quality?' and any later adopted guidance and policy.
3. The development shall not be implemented other than following the approved scheme, unless previously agreed in writing by the Local Planning Authority.

Reason: To ensure the development does not raise local environment impacts and pollution.

- 10 All Non-Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site

preparation and construction phases shall comply with the emission standards set out in chapter 7 of the GLA's supplementary planning guidance "Control of Dust and Emissions During Construction and Demolition" dated July 2014 (SPG), or subsequent guidance. Unless it complies with the standards set out in the SPG, no NRMM shall be on site, at any time, whether in use or not, without the prior written consent of the local planning authority. The developer shall keep an up to date list of all NRMM used during the demolition, site preparation and construction phases of the development on the online register at <https://nrmm.london/>

Reason: To manage and prevent further deterioration of existing low quality air across London in accordance with London Plan Policies SI1(B)(1c) and SI1(B)(2d).

- 11 Prior to the occupation of any part of the development hereby approved, a comprehensive delivery and service plan, to manage, co-ordinate and minimise all deliveries and services, including waste services, to all parts of the development, shall be submitted to and approved in writing by the Local Planning Authority. Where developers are encouraged to consolidate Delivery and Service Plans with other neighbouring premises servicing neighbouring properties. The scheme shall provide details of measures to be implemented and maintained to minimise and manage all deliveries and services to all parts of the development to. Central pick-up locations must be agreed, and personal deliveries discouraged. The development shall not be occupied other than in accordance with the approved scheme.

Reason: To reduce the number of vehicles and emissions from vehicles for deliveries and services and to mitigate the impact of the development upon local air quality.

- 12 Prior to the commencement of development, a detailed scheme for the provision of surface and foul water drainage shall be submitted to and approved in writing by the local planning authority for both phases of the development. The drainage scheme will dispose of surface water by means of a sustainable drainage system (SuDS) at the agreed runoff rate (no more than 2 l/s, with no less than 15.8m³ of attenuation volume) and include three green roofs and permeable paving in accordance with drainage hierarchy contained within the London Plan Policy (SI 12, SI13 and SPG) and the advice contained within the National SuDS Standards.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI13.

- 13 Prior to occupation of the development a detailed SuDS maintenance plan shall be submitted to and approved in writing by the local planning authority. This must include the maintenance provider and be included as part of the general maintenance of the development.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI13.

- 14 Prior to the commencement of development, the detailed design and specification for the green roofs shall be submitted to and approved in writing by the Local Planning Authority. The design shall be carried out as approved, retained and maintained by the applicant in perpetuity thereafter.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

- 15 Prior to occupation of the development a detailed SuDS maintenance plan shall be submitted to and approved in writing by the local planning authority. This must include the maintenance provider and be included as part of the general maintenance of the development.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

- 16 The development shall not be occupied until the existing redundant crossover/s have been removed by raising the kerb and reinstating the footway in accordance with the requirements of the Highway Authority.

Reason: In the interests of the safety of pedestrians and vehicles and to comply with the following Development Plan policies for Merton: policies CS18 and CS20 of Merton's Core Planning Strategy 2011 and policies DM T2, T3, T4 and T5 of Merton's Sites and Policies Plan 2014.

- 17 Prior to the commencement of the development hereby permitted, a Construction Logistics Plan shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented prior to the first occupation of the development hereby permitted and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

- 18 Development shall not commence until a working method statement has been submitted to and approved in writing by the Local Planning Authority to accommodate:

- (i) Parking of vehicles of site workers and visitors;
- (ii) Loading and unloading of plant and materials;
- (iii) Storage of construction plant and materials;
- (iv) Wheel cleaning facilities
- (v) Control of dust, smell and other effluvia;
- (vi) Control of surface water run-off.

No development shall be carried out except in full accordance with the approved method statement.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

- 19 No demolition or construction work or ancillary activities such as deliveries shall take place before 8am or after 6pm Mondays - Fridays inclusive, before 8am or after 1pm on Saturdays or at any time on Sundays or Bank Holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policies D14 and T7 of the London Plan 2021 and policy DM EP2 of Merton's Sites and Policies Plan 2014.

- 20 No development shall take place until full details of a landscaping and planting scheme has been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved before the commencement of the use or the occupation of any building hereby approved, unless otherwise agreed in writing by the Local Planning Authority. The details shall include on a plan, full details of the size, species, spacing, quantities and location of proposed plants, together with any hard surfacing, means of enclosure, and indications of all existing trees, hedges and any other features to be retained, and measures for their protection during the course of development.

Reason: To enhance the appearance of the development in the interest of

the amenities of the area, to ensure the provision sustainable drainage surfaces and to comply with the following Development Plan policies for Merton: policies G7 and D8 of the London Plan 2021, policies CS13 and CS16 of Merton's Core Planning Strategy 2011 and policies DM D2, DM F2 and DM O2 of Merton's Sites and Policies Plan 2014.

- 21 Not less than 10% of the dwelling units hereby permitted shall be constructed shall be wheelchair accessible throughout or easily adaptable for residents who are wheelchair users and shall be retained as such unless otherwise agreed in writing with the Local planning Authority.

Reason: To ensure the housing stock addresses the housing needs of disabled persons and to comply with the following Development Plan policies for Merton: policies D7 and H12 of the London Plan 2021, policy CS8 of Merton's Core Planning Strategy 2011 and policy DM D2 of Merton's Sites and Policies Plan 2014.

- 22 Level access or a ramp at a gradient of not more than 1:12 and no less than a 900mm door width at the threshold to the threshold to the principal entrance to the premises shall be provided before the development hereby permitted is first occupied or brought into use and shall be retained thereafter.

Reason: To ensure suitable access for persons with disabilities and to comply with the following Development Plan policies for Merton: policies D7 and H12 of the London Plan 2021, policy CS8 of Merton's Core Planning Strategy 2011 and policy DM D2 of Merton's Sites and Policies Plan 2014.

- 23 No piling shall take place until a PILING METHOD STATEMENT (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimize the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to significantly impact / cause failure of local underground sewerage utility infrastructure.

- 24 No piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimize the potential for damage to subsurface water infrastructure, and the programme for the works) has been submitted to and approved in writing by the local

planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground water utility infrastructure. Piling has the potential to impact on local underground water utility infrastructure.

- 25 L2 Sustainability - Pre-Commencement (New build residential)
- 26 L6 BREEAM - Pre-Commencement (New build non residential)
- 27 INFORMATIVE
Details of wheelchair accessible standards can be found in 'Wheelchair Housing Design Guide' (Second Edition, 2006) by Stephen Thorpe.
- 28 INFORMATIVE
Details of the BREEAM assessment and a list of approved assessors can be found at www.breeam.org
- 29 INFORMATIVE
You are advised to contact the Council's Highways team on 020 8545 3700 before undertaking any works within the Public Highway to obtain the necessary approvals and/or licences. Please be advised that there is a further charge for this work. If your application falls within a Controlled Parking Zone this has further costs involved and can delay the application by 6 to 12 months.
- 30 INFORMATIVE
Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be co-ordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Merton. Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with the London Borough of Merton, Network Coordinator, (telephone 020 8545 3976). This must take place at least one month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are co-ordinated to take place wherever possible at the same time.
- 31 INFORMATIVE

This planning permission contains certain conditions precedent that state 'before development commences' or 'prior to commencement of any development' (or similar). As a result these must be discharged prior to ANY development activity taking place on site. Commencement of development without having complied with these conditions will make any development unauthorised and possibly subject to enforcement action such as a Stop Notice.

32 INFORMATIVE

This permission creates one or more new units which will require a correct postal address. Please contact the Street Naming & Numbering Officer at the London Borough of Merton

Street Naming and Numbering (Business Improvement Division)
Corporate Services
7th Floor, Merton Civic Centre
London Road
Morden
SM4 5DX
Email: street.naming@merton.gov.uk

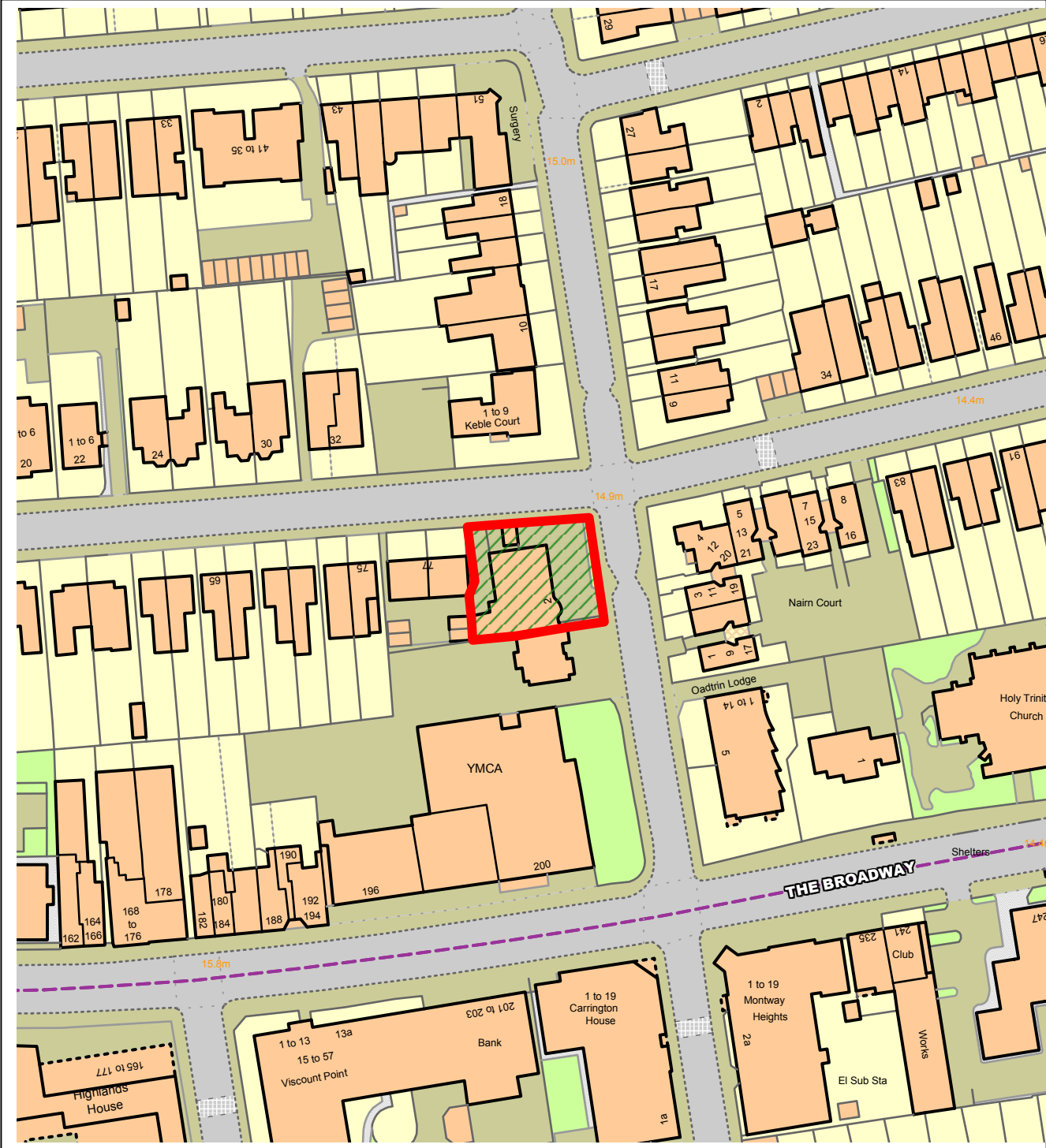
33 INFORMATIVE

A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailingtrade.effluent@thameswater.co.uk. Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.

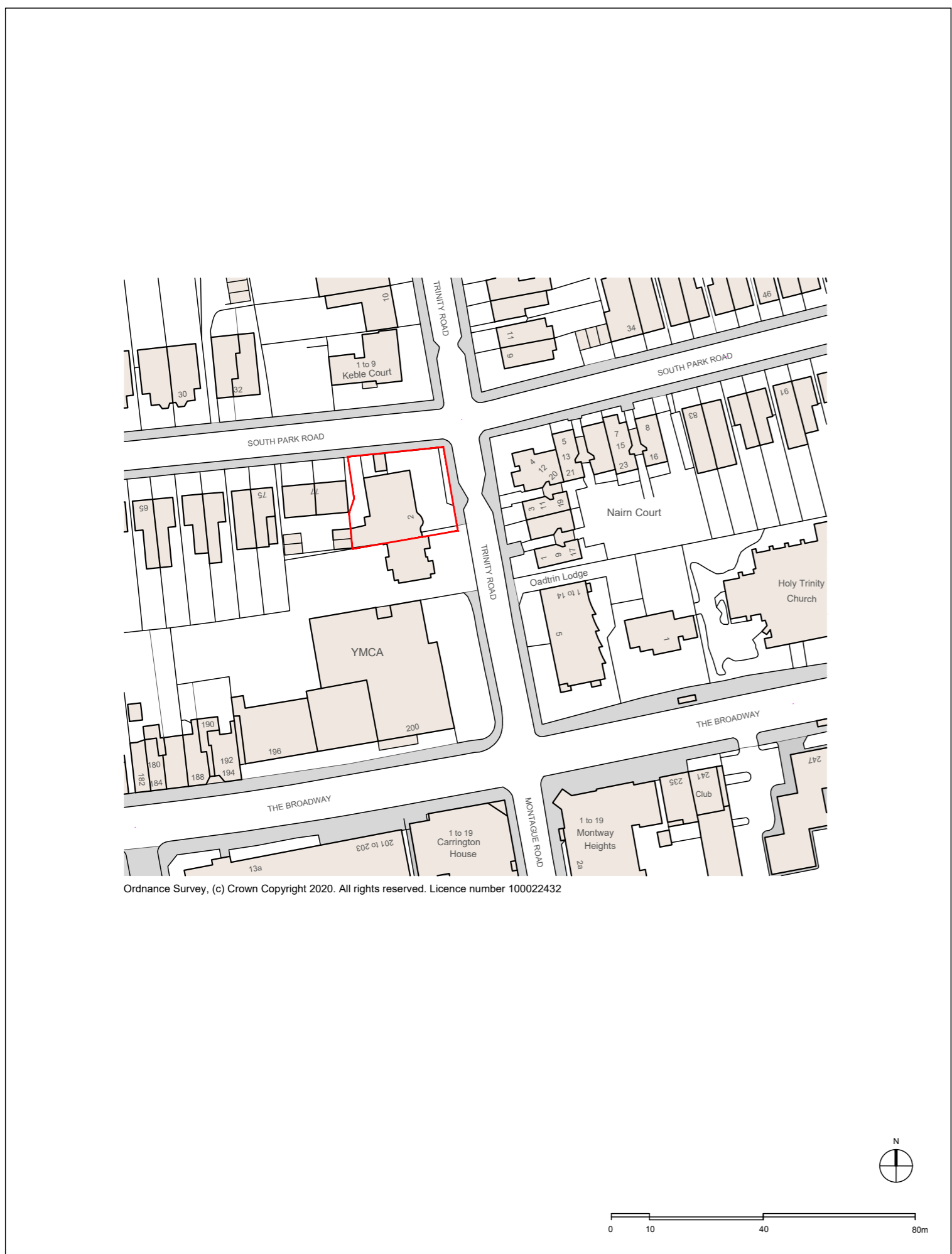
34 INFORMATIVE

Details of Lifetime Homes Standards can be found at www.lifetimehomes.org.uk

NORTHGATE SE GIS Print Template



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Site Location Plan 1.1250



Site Plan 1.500

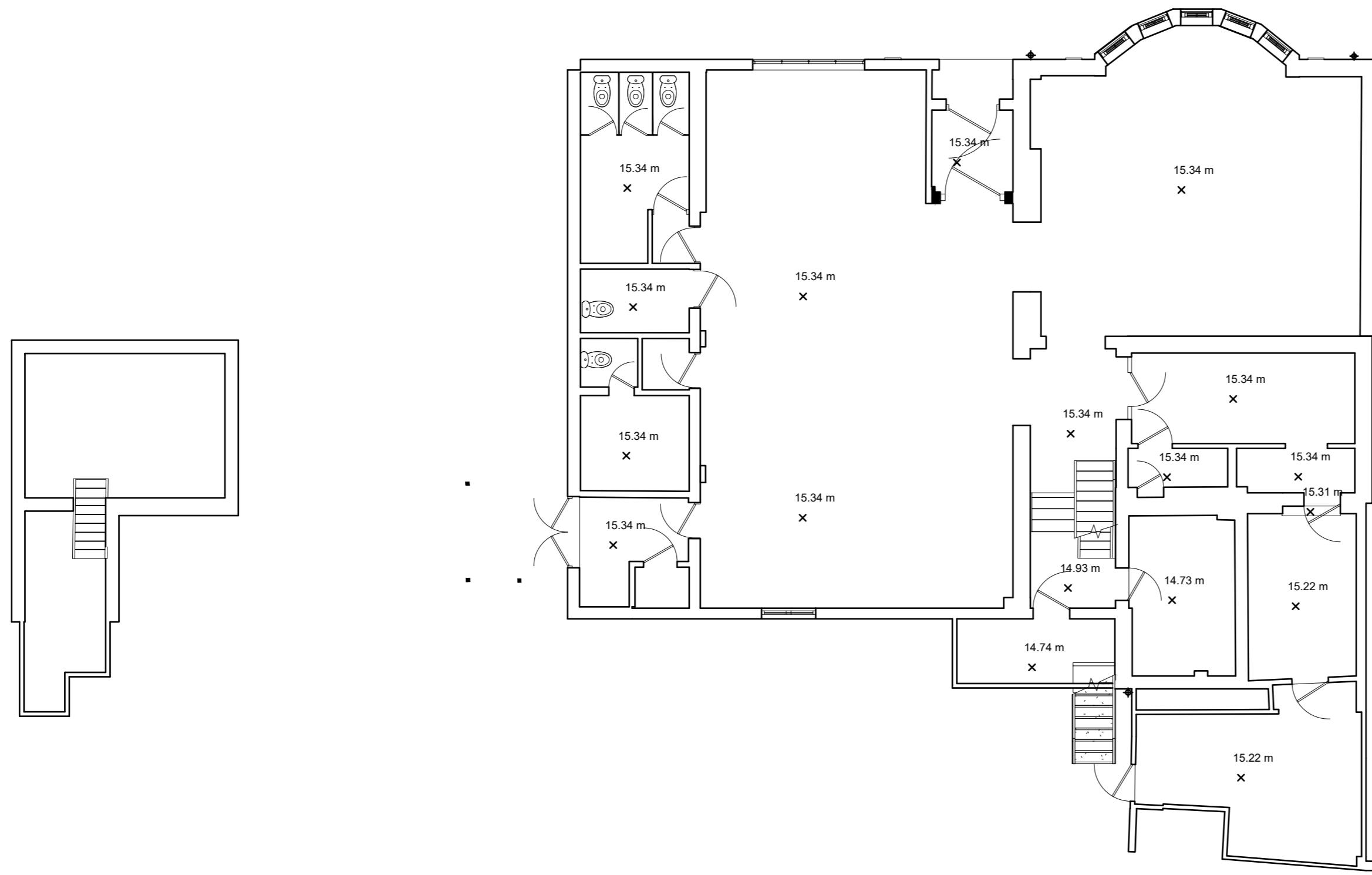
KEY
 — Site Boundary
 — Proposed YMCA Development

A1

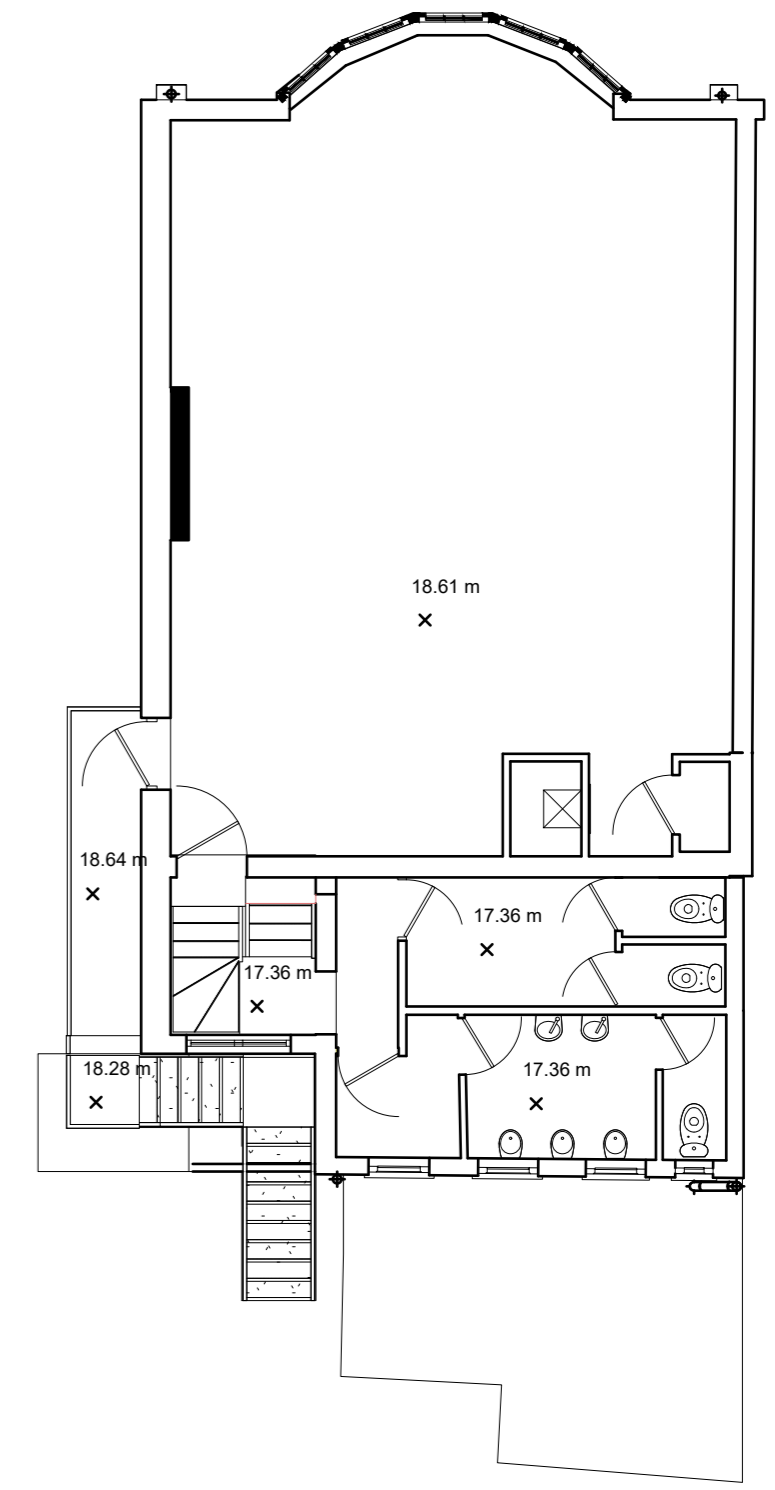
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	A	14.07.2021	Issued for Planning	July 2021	2 Trinity Road Wimbledon		TRH	241	
3W Architecture Limited registered in England.				Scale @ A1	Checked	Title			
Company no. 3132871 Registered Office The Pines Boars Head East Sussex				1:500 & 1.1250	SW	Site Location and Site Plans			
				Status		Dwg. no.	Rev.		
				Planning		241(05)001	A		



Site Plan 1.2000

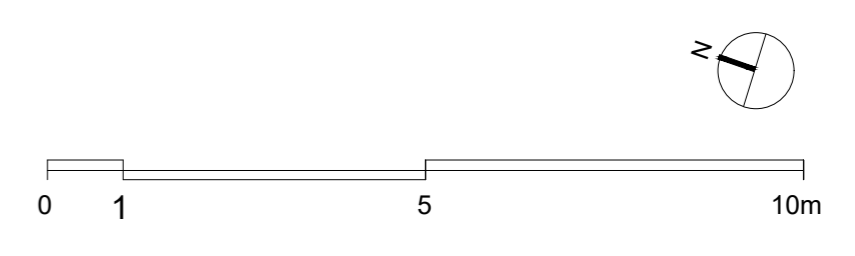


Existing Ground Floor Plan 1.100

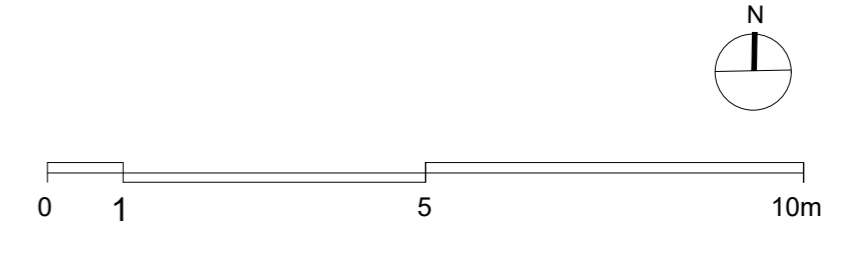


Existing First Floor Plan 1.100

Existing Basement Plan 1.100

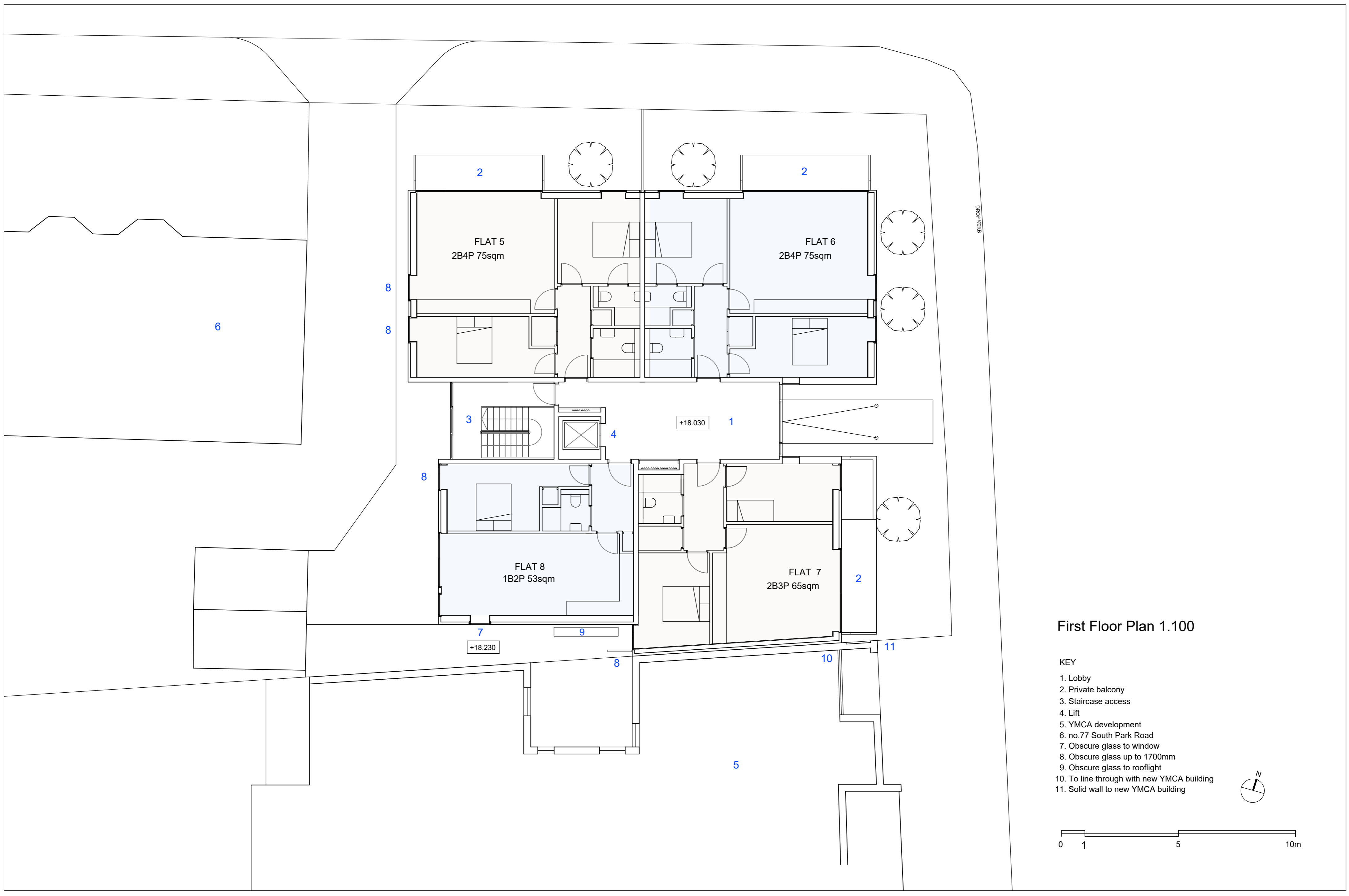


Ground Floor Topographic Plan 1.100



A1

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	A	14.08.2021	Issued for Planning	July 2021	2 Trinity Road Wimbledon	TRH	241	
3W Architecture Limited registered in England.				Scale @ A1 1:100	Checked SW	Title Existing Building Plans		
Company no. 3132871 Registered Office The Pines Boars Head East Sussex				Status Planning	Dwg. no. 241(12)011	Rev. A		



A1

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	A	14.07.2021	Issued for Planning	July 2021	2 Trinity Road Wimbledon	TRH	241
				Scale @ A1 1:100	Checked SW	Title Ground and First Floor Plans	Rev. A
			Status Planning		Dwg. no. 241(10)001		





Third Floor Plan 1.100

KEY

1. Lobby
2. Private balcony
3. Staircase access
4. Lift
5. YMCA development
6. no.77 South Park Road
7. Obscure glass to screen
8. To line through with new YMCA building
9. Solid wall to new YMCA building
10. Obscure glass to window



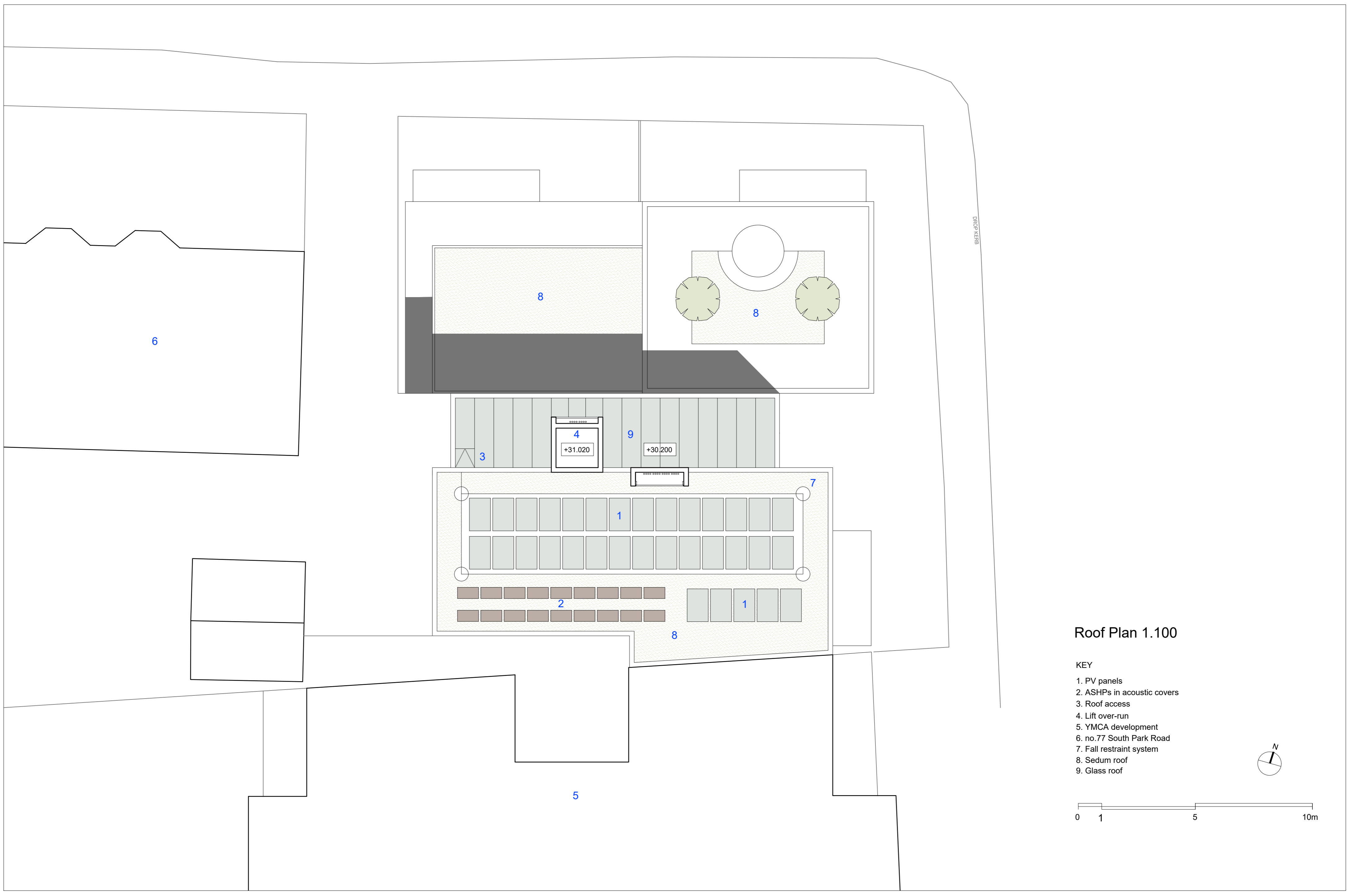
Second Floor Plan 1.100

KEY

1. Lobby
2. Private balcony
3. Staircase access
4. Lift
5. YMCA development
6. no.77 South Park Road
7. Obscure glass to window
8. Obscure glass up to 1700mm
9. Rooflight
10. To line through with new YMCA building
11. Solid wall to new YMCA building

A1

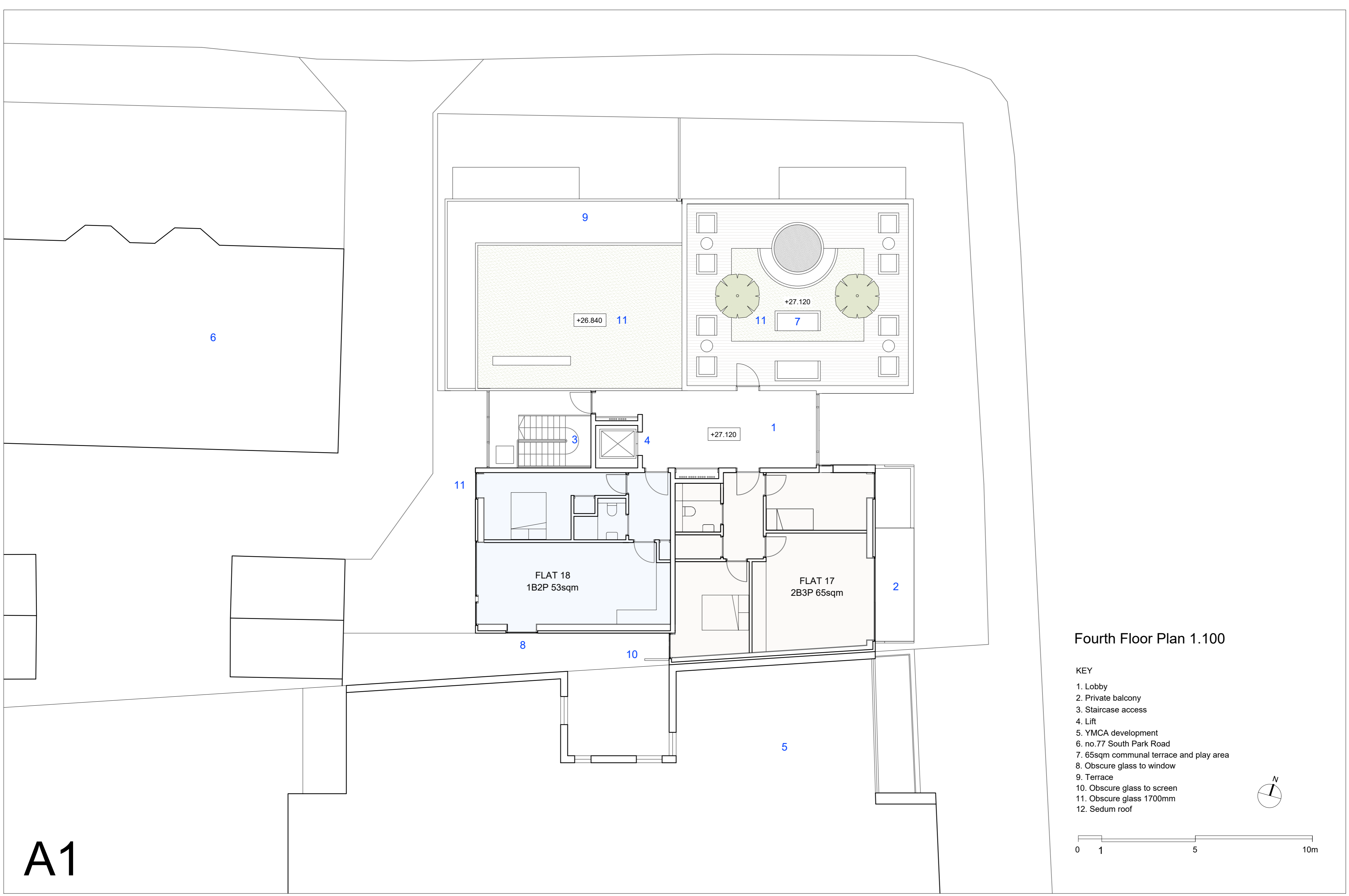
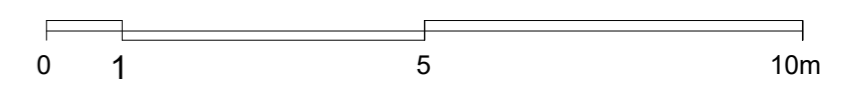
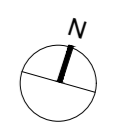
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3W Architecture Limited registered in England.				Scale @ A1	Checked	Title	Rev.	
Company no. 3132871 Registered Office The Pines Boars Head East Sussex				1:100	SW	Second and Third Floor Plans	A	
				Status	Dwg. no.			
				Planning	241(10)002			



Roof Plan 1.100

KEY

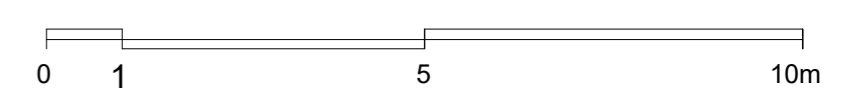
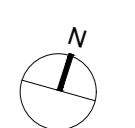
- 1. PV panels
- 2. ASHPs in acoustic covers
- 3. Roof access
- 4. Lift over-run
- 5. YMCA development
- 6. no.77 South Park Road
- 7. Fall restraint system
- 8. Sedum roof
- 9. Glass roof



Fourth Floor Plan 1.100

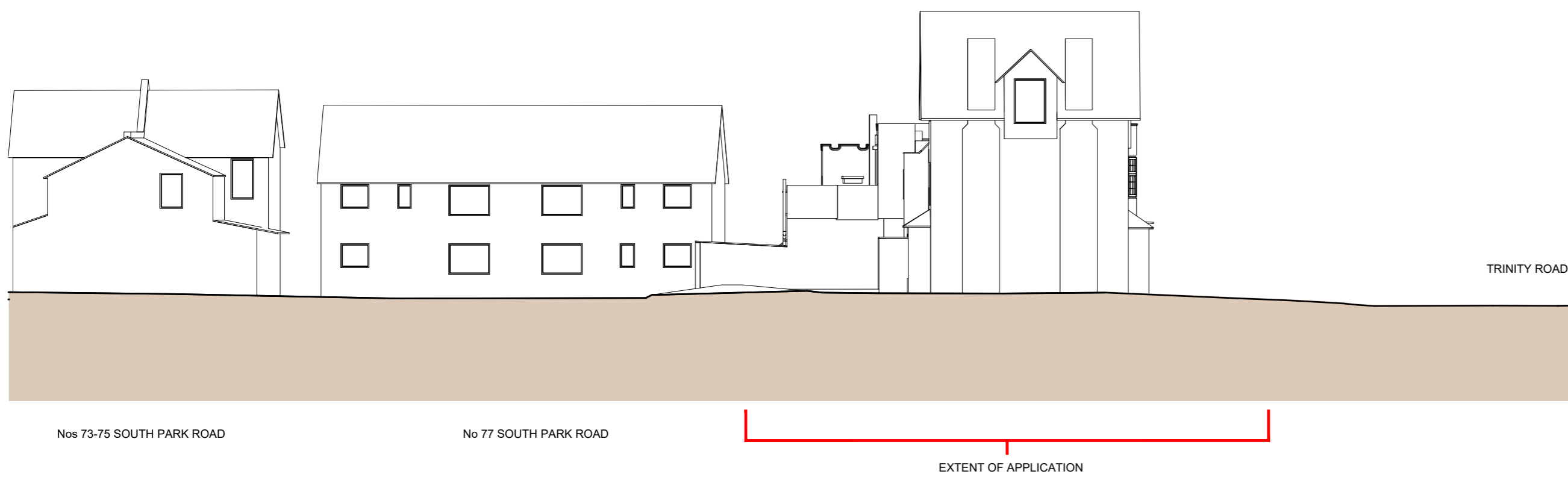
KEY

- 1. Lobby
- 2. Private balcony
- 3. Staircase access
- 4. Lift
- 5. YMCA development
- 6. no.77 South Park Road
- 7. 65sqm communal terrace and play area
- 8. Obscure glass to window
- 9. Terrace
- 10. Obscure glass to screen
- 11. Obscure glass 1700mm
- 12. Sedum roof



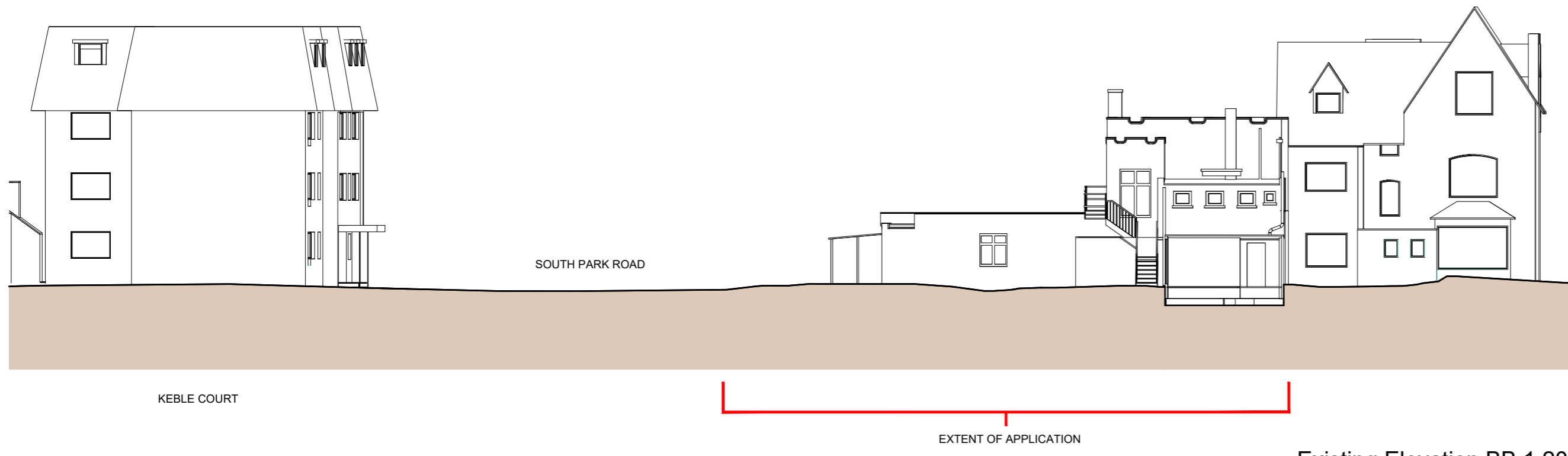
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3W Architecture Limited registered in England.				Scale @ A1	Checked			
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				Planning	241(10)003	A		

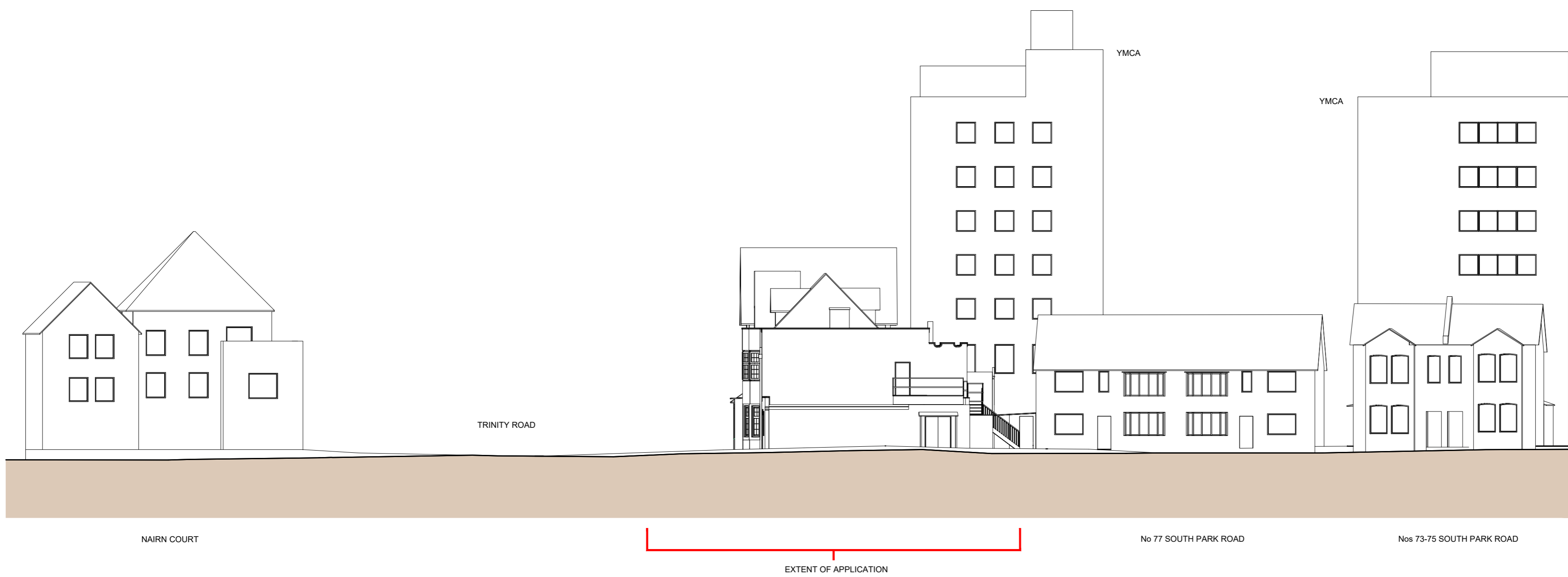


Existing Elevation AA 1.200

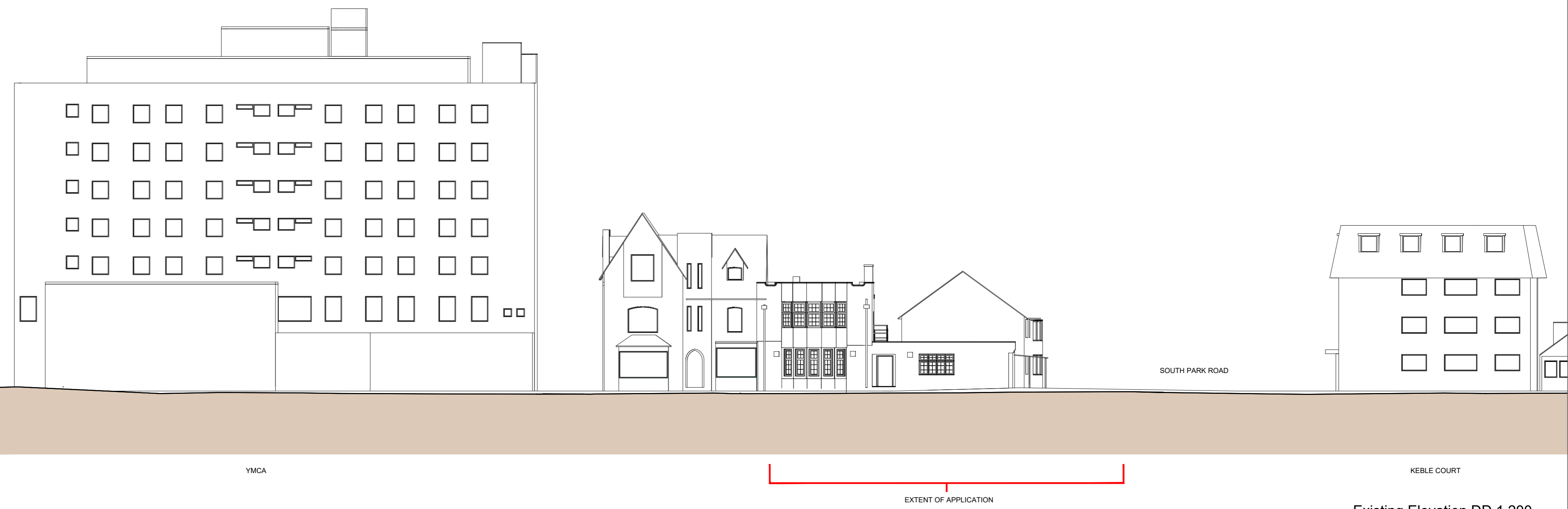
Site Plan 1.2000



Existing Elevation BB 1.200



Existing Elevation CC 1.200



Existing Elevation DD 1.200

A1



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	A	14.07.2021	Issued for Planning	July 2021	2 Trinity Road Wimbledon	TRH	241	
				Scale @ A1 1:200	Checked SW	Title Existing Elevations	Rev. A	
				Status Planning	Dwg. no. 241(12)010			



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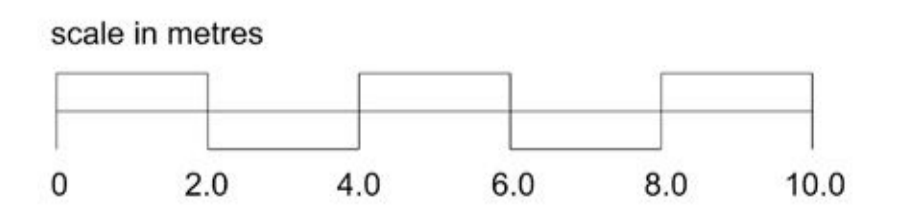
YMCA Development

2 Trinity Road

South Park Road

Keble Court

East Elevation



Rev	Date	Amendment	Rev	Date	Amendment	Rev	Date	Amendment	Rev	Date	Amendment	Date	Project	Job Ref.	Job no.
A	22.07.2021	Issued for planning										19.07.2021	2 Trinity Road - Wimbledon	TRH	241
© 3W Architecture Limited. All rights reserved.												Scale @ A1	Checked	Title	
3W Architecture Limited registered in England.												1:100 (200 @ A3)		East Elevation	
Company no. 3132871 Registered Office The Pines Boars Head East Sussex												Status	Protocol	Dwg. No.	Rev.
												Planning		241(11)001	A
												Architect			
												3W	Studio 1S.09 - Barley Mow Business Centre - 10 Barley Mow Passage - London - 020 7835 5525 - www.3w.org		





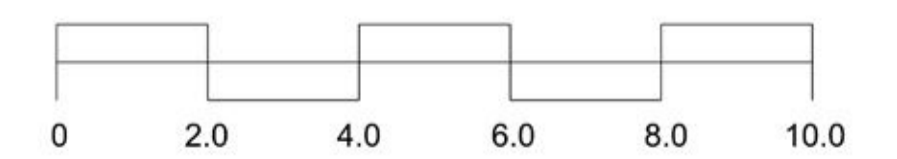
Trinity Road

2 Trinity Road

South Park Road

South Park Road

North Elevation



Rev	Date	Amendment	Rev	Date	Amendment	Rev	Date	Amendment	Rev	Date	Amendment
A	22.07.2021	Issued for planning									

© 3W Architecture Limited. All rights reserved.	Date 19.07.2021	Project 2 Trinity Road - Wimbledon	Job Ref. TRH	Job no. 241
3W Architecture Limited registered in England.	Scale @ A1 1:100 (200 @ A3)	Title North Elevation		
Company no. 3132871 Registered Office The Pines Boars Head East Sussex	Status Planning	Protocol	Dwg. No. 241(11)002	Rev. A
	Architect			
	3W Studio 1S.09 - Barley Mow Business Centre - 10 Barley Mow Passage - London - 020 7835 5525 - www.3w.org			



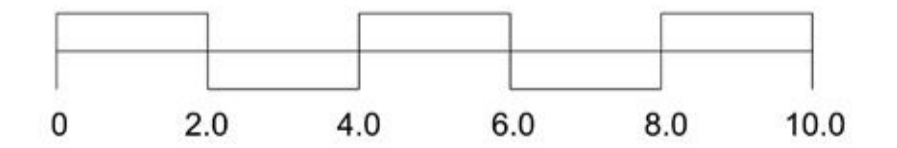
South Park Road

2 Trinity Road

YMCA Development

West Elevation

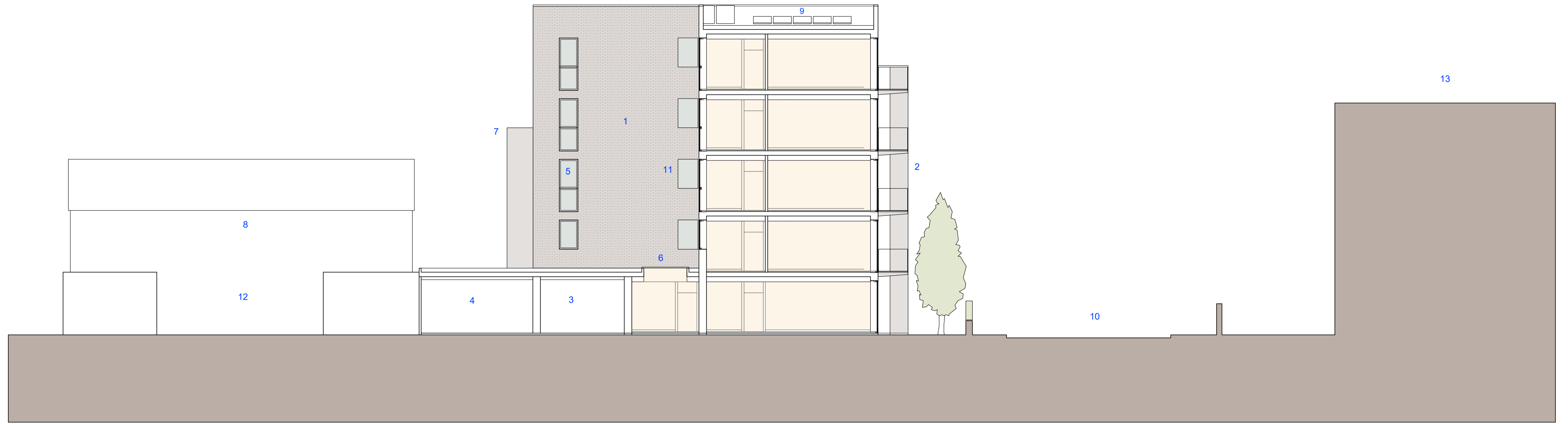
scale in metres



Rev	Date	Amendment	Rev	Date	Amendment	Rev	Date	Amendment	Rev	Date	Amendment	Date	Project	Job Ref.	Job no.
A	22.07.2021	Issued for planning										19.07.2021	2 Trinity Road - Wimbledon	TRH	241
												Scale @ A1	Title		
												1:100 (200 @ A3)	West Elevation		
												Status	Protocol	Dwg. No.	Rev.
												Planning		241(11)003	A
												Architect			
												3W	Studio 1S.09 - Barley Mow Business Centre - 10 Barley Mow Passage - London - 020 7835 5525 - www.3w.org		

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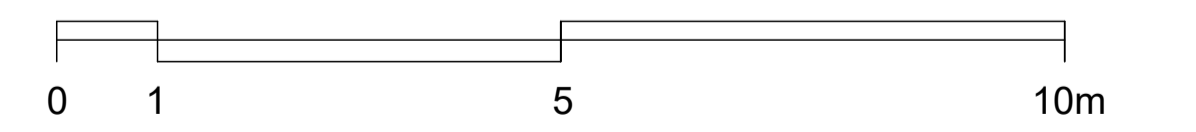




KEY

- 1. Stock brick
- 2. Balconies with privacy fins
- 3. Bin store
- 4. Bike store
- 5. Obscure glass to windows
- 6. Obscure glass to rooflight
- 7. Height on South Park Road
- 8. No 77 South Park Road
- 9. All renewable plant below parapet level
- 10. Trinity Road
- 11. Obscure glass to fins
- 12. Garages and car park to no. 77 South Park Road
- 13. Naim Court

A1



	Rev	Date	Amendment	Date	Project	Job Ref.	Job no.
	A	27.11.2020	First Issue	July 2021	2 Trinity Road Wimbledon	TRH	241
© 3W Architecture Limited. All rights reserved.				Scale @ A1	Checked	Title	
3W Architecture Limited registered in England.				1:100	SW	Section AA	
Company no. 3132871 Registered Office The Pines Boars Head East Sussex				Status	Dwg. no.	Rev.	
				Planning	241(11)001	A	
				Architect			
				tel. 020 7835 5552 fax. 020 7835 5525 email. mail@3w.org			



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Committee: Planning Applications

Date: 28th April 2021

Subject: Planning Appeal Decisions

Lead officer: Head of Sustainable Communities

Lead member: Chair, Planning Applications Committee

Recommendation:

That Members note the contents of the report.

1. PURPOSE OF REPORT AND EXECUTIVE SUMMARY

- 1.1 For Members' information recent decisions made by Inspectors appointed by the Secretary of State for Communities and Local Government in respect of recent Town Planning Appeals are set out below.
- 1.2 The relevant Inspectors decision letters are not attached to this report but can be viewed by following each individual link. Other agenda papers for this meeting can be viewed on the Committee Page of the Council Website via the following link:

[LINK TO COMMITTEE PAGE](#)

DETAILS

Application Number **18/P0947**
Site: 201 Manor Way, Mitcham CR4 1EN
Development: PRIOR APPROVAL FOR A SINGLE STOREY REAR EXTENSION
LPA Decision: REFUSED (Delegated Decision)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 31ST March 2022

[LINK TO DECISION](#)

Application Number **19/P4183**
Site: Dundonald Recreation Ground, Dundonald Road, Wimbledon
Development: ERECTION OF A TEMPORARY BUILDING TO PROVIDE
 COMMUNITY SPACE, TENNIS CLUB + CAFE AND ERECTION OF
 SEPARATE TEMPORARY TOILET FACILITIES
LPA Decision: REFUSED @ PAC (Committee Decision)
Appeal Decision: **ALLOWED**
Date of Appeal Decision: 11th March 2022

[LINK TO DECISION](#)

Application Number **21/P0943**
Site: 7 Christchurch Close, Colliers Wood SW19 2NZ
Development: DEMOLITION OF EXISTING GARAGE AND ERECTION OF 1 x 2
 BEDROOM TWO STOREY DETACHED DWELLINGHOUSE
LPA Decision: REFUSED @ PAC (Committee Decision)
Appeal Decision: **ALLOWED**
Date of Appeal Decision: 16th March 2022

[LINK TO DECISION](#)

Application Number **21/P1988**
Site: 38 Lyveden Road, Tooting SW17 9DU
Development: CONVERSION OF EXISTING DWELLINGHOUSE INTO 5 x SELF-
 CONTAINED FLATS, INCLUDING ERECTION OF GROUND AND
 FIRST FLOOR EXTENSIONS, A REAR ROOF EXTENSION,
 EXCAVATION AND BASEMENT EXTENSION, ASSOCIATED
 LANDSCAPING (INCLUDING DEMOLITION OF EXISTING
 GARDEN STRUCTURES), PLUS CYCLE AND REFUSE
 STORAGE.
LPA Decision: REFUSED @ PAC (Committee Decision)
Appeal Decision: **ALLOWED**
Date of Appeal Decision: 11th March 2022

[LINK TO DECISION](#)

Application Number **21/P2135**
Site: 41 Eastfields Road, Mitcham CR4 2LS
Development: DEMOLITION OF EXISTING BUILDING AND ERECTION OF A PART 2 STOREY / PART THREE STOREY DETACHED RESIDENTIAL BLOCK CREATING 8 x SELF-CONTAINED FLATS. WITH CYCLE & REFUSE STORAGE AND LANDSCAPING.
LPA Decision: REFUSED (Delegated Decision)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 15th March 2022

[LINK TO DECISION](#)

Application Number **21/P2344**
Site: 61 Woodland Way, Morden SM4 4DS
Development: ERECTION OF A TWO STOREY SIDE EXTENSION AND ROOF TERRACE.
LPA Decision: REFUSED (Delegated Decision)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 23rd March 2022

[LINK TO DECISION](#)

Application Number **21/P2525 and 21/P2526**
Site: 299 Cannon Hill Lane, Raynes Park SW20 9HQ
Development: ERECTION OF A SINGLE STOREY REAR EXTENSION; FIRST FLOOR SIDE EXTENSION; HIP TO GABLE WITH REAR DORMER ROOF EXTENSION; INSTALLATION OF 2X ROOFLIGHTS ON THE FRONT ROOFSLOPE AND 1X FOLD-OUT ROOF BALCONY ROOFLIGHT; 1X NEW WINDOW ON SIDE (NORTH) ELEVATION.
LPA Decision: REFUSED (Delegated Decision)
Appeal Decision: **ALLOWED**
Date of Appeal Decision: 31ST March 2022

[LINK TO DECISION](#)

Application Number **21/P3034**
Site: 98 Graham Road, Wimbledon SW19 3SS
Development: ERECTION OF A PRIVACY SCREEN AROUND EDGES OF FIRST FLOOR FLAT ROOF TO CREATE A SCREENED ROOF TERRACE.
LPA Decision: REFUSED (Delegated Decision)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 25th March 2022

[LINK TO DECISION](#)

Application Number **21/P3356**
Site: 67 Oxford Avenue, Wimbledon Chase SW20 8LS
Development: ERECTION OF A SECOND FLOOR REAR EXTENSION ABOVE EXISTING OUTRIGGER
LPA Decision: REFUSED (Delegated Decision)
Appeal Decision: **ALLOWED**
Date of Appeal Decision: 25th March 2022

[LINK TO DECISION](#)

Alternative options

- 3.1 The appeal decision is final unless it is successfully challenged in the Courts. If a challenge is successful, the appeal decision will be quashed and the case returned to the Secretary of State for re-determination. It does not follow necessarily that the original appeal decision will be reversed when it is re-determined.
- 3.2 The Council may wish to consider taking legal advice before embarking on a challenge. The following applies: Under the provision of Section 288 of the Town & Country Planning Act 1990, or Section 63 of the Planning (Listed Buildings and Conservation Areas) Act 1990, a person or an establishment who is aggrieved by a decision may seek to have it quashed by making an application to the High Court on the following grounds: -
1. That the decision is not within the powers of the Act; or

2. That any of the relevant requirements have not been complied with; (relevant requirements means any requirements of the 1990 Act or of the Tribunal's Land Enquiries Act 1992, or of any Order, Regulation or Rule made under those Acts).

1 CONSULTATION UNDERTAKEN OR PROPOSED

- 1.1. None required for the purposes of this report.

2 TIMETABLE

- 2.1. N/A

3 FINANCIAL, RESOURCE AND PROPERTY IMPLICATIONS

- 3.1. There are financial implications for the Council in respect of appeal decisions where costs are awarded against the Council.

4 LEGAL AND STATUTORY IMPLICATIONS

- 4.1. An Inspector's decision may be challenged in the High Court, within 6 weeks of the date of the decision letter (see above).

5 HUMAN RIGHTS, EQUALITIES AND COMMUNITY COHESION IMPLICATIONS

- 5.1. None for the purposes of this report.

6 CRIME AND DISORDER IMPLICATIONS

- 6.1. None for the purposes of this report.

7 RISK MANAGEMENT AND HEALTH AND SAFETY IMPLICATIONS

- 7.1. See 6.1 above.

8 BACKGROUND PAPERS

- 8.1. The papers used to compile this report are the Council's Development Control service's Town Planning files relating to the sites referred to above and the agendas and minutes of the Planning Applications Committee where relevant.

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Committee: Planning Applications Committee

Date: 28th April 2022

Agenda item:

Wards: All

Subject: PLANNING ENFORCEMENT - SUMMARY OF CASES

Lead officer: HEAD OF SUSTAINABLE COMMUNITIES

Lead member: CABINET MEMBER FOR REGENERATION, HOUSING AND
TRANSPORT COUNCILLOR MARTIN WHELTON

COUNCILLOR DAVE WARD, CHAIR, PLANNING APPLICATIONS COMMITTEE

Contact Officers Ray Littlefield: 0208 545 3911
Ray.Littlefield@merton.gov.uk
Raymond Yeung: 0208 545 4352
Raymond.Yeung@merton.gov.uk

Recommendation:

That Members note the contents of the report.

1. Purpose of report and executive summary

This report details a summary of casework being dealt with by the Planning Enforcement Team and contains figures of the number of different types of cases being progressed, with brief summaries of all new enforcement notices and the progress of all enforcement appeals.

Current Enforcement Cases:	580	¹ (600)	New Appeals:	(0)	(0)
New Complaints	39	(34)	Instructions to Legal	1	(1)
Cases Closed	305	(26)	Existing Appeals	2	(2)
New Enforcement Notices Issued			TREE ISSUES		
Breach of Condition Notice:	0		Tree Applications Received	62	(79)
New Enforcement Notice issued	1	(0)	% Determined within time limits:	100%	
S.215: ³	1		High Hedges Complaint	0	(1)
Others (PCN, TSN)	0	(0)	New Tree Preservation Orders (TPO)	4	(3)
Total	2	(1)	Tree Replacement Notice	0	
Prosecutions: (instructed)	0	(0)	Tree/High Hedge Appeal	0	(0)

Note (**figures are for the period from (from 19th March 2022 to 19th April 2022)**). The figure for current enforcement cases was taken directly from M3 crystal report.

¹ Totals in brackets are previous month's figures

² confirmed breach but not expedient to take further action.

³ S215 Notice: Land Adversely Affecting Amenity of Neighbourhood.

It should be noted that due to the pandemic the Planning Inspectorate have over a year's backlog of planning enforcement appeals to determine.

2.0 Recent Enforcement Actions

70 Linkway, SW20 9AZ. Unauthorised hardsurfacing of front garden.

1st site visit: 30.11.21 – Before



Site visit: 19.1.22 – AFTER photo



The breach has now been rectified the hardstanding or cement has been removed and the front garden has been reinstated with a grassed area and a wooden boundary fence

Land to the rear of 42 Tamworth Lane, Mitcham, CR4 1DA. This is concerning a s215 notice served on untidy land. A s215 notice was issued on 10th May 2021. This notice requires compliance at the end of July 2021 requiring the Land to be tidied up / cleared. The Council have now taken Direct Action and cleared the land.

The Land is again being fly tipped a further s215 Notice is to be issued, to include enclosing the Land and clearing the untidy / overgrown Land.

100 Garth Road, Morden, SM4 4LR. Relates to the unauthorised erection of a self-contained residential unit on top of an existing garage. An enforcement notice has been served dated 28th March 2022, the Notice will take effect on 2nd May 2022 with a 3 months compliance period unless an appeal is submitted. The notice requires: Completely demolish the Unit or Restore that part of the property to its condition prior to the breach of planning control by complying with approved drawing number E-1672-PJ-03A planning permission 17/P2214.

Land at 225-231 Streatham Road, SW16.

A Temporary Stop Notice was issued on 2nd February 2022 requiring the immediate cessation of use of the Land as a car wash. The notice took immediate effect, and the unauthorised use was ceased, and the Notice fully complied with.

Parkside House, 52/54 High Street, Wimbledon, London SW19 5AY. Commercial Unit on Land to the rear. A Temporary Stop Notice was issued on 31st December 2021 relating to works being undertaken creating an unauthorised rear ground floor extension. The Notice came into immediate effect, the Notice will cease to have an effect after 27th January 2022. Works Stopped, Notice complied with.

52B Russell Road, Wimbledon, London, SW19 1QL. This is regarding the erection of a 2 metre boundary fence, facing the highway, which had a retrospective planning application submitted ref: 20/P2317 and was refused. The applicant appealed the decision to the Planning Inspectorate. The appeal was dismissed on 14th June 2021. An enforcement notice was issued on 13th September 2021 to remove the fence. The Notice took effect on 15th October 2021 with a one calendar month period of time for compliance. This Notice has now been fully complied with.

193 London Road, CR4 2JD. This is concerning a s215 notice served on untidy land. The Land is actively being cleared.

31 Edgehill Road, Mitcham, CR4 2HY. This is concerning a raised platform/garden that has been raised by approximately 90cm. An enforcement notice has been served to remove the raised platform and reduce the garden level by 90cm. The notice would have taken effect on 18/12/19, with a compliance date of 18/03/20, however an appeal has been submitted and is underway.

Successful Prosecution case

7 Streatham Road, Mitcham, CR4 2AD

Direct action is being in process for the remedial works following non-compliance to enforcement notices. As previously mentioned, The Council served two enforcement notices on 6th June 2019, requiring the outbuilding to be demolished and to clear debris and all other related materials.

The second enforcement notice is for an unauthorised front, side and rear (adjacent to Graham Road) dormer roof extensions. An appeal was lost for the dormers to be considered permitted development, the notice requires the owner to demolish the unauthorised front, side and rear roof dormer extensions (adjacent to Graham Road) and to clear debris and all other related materials. Both Notices came into effect on 8th July 2019 unless appeals were made before this date. No appeals were lodged.

The compliance date of the Enforcement Notice relating to the outbuilding to be demolished and to clear debris and all other related materials has now passed without compliance. The second enforcement notice was not complied with and now prosecution proceedings are being undertaken.

The plea hearing has now taken place at Lavender Hill Magistrates Court, where the defendant pleaded not guilty and the second hearing is due on the 14th January 2020.

A second hearing was held on 14th January 2020, and adjourned until 4th February 2020 in order for the defendant to seek further legal advice.

The defendant again appeared in court and pleaded not guilty, a trial date was set for 21st May 2020. Due to the Covid-19 pandemic this has been postponed. The case has been listed for a 'non-effective' hearing on Tuesday 14 July 2020, where a new trial date will be set.

This was postponed until another date yet to be given. The Council has now instructed external Counsel to prosecute in these matters.

The next 'non-effective' hearing date is 2nd October 2020. This date has been re-scheduled to 27th November 2020. This was again re-scheduled to 4th January 2021. Outcome not known at the time of compiling this report.

A trial date has now been set for 28th and 29th April 2021.

At trial the defendant changed his plea from not guilty to guilty on the two charges of failing to comply with the two Planning Enforcement Notices, however due to the current appeals with the Planning Inspectorate relating to two planning application appeals associated with the two illegal developments, sentencing was deferred until 7th October 2021 at Wimbledon Magistrates Court.

The two planning appeals were dismissed dated 5th October 2021.

Sentencing was again deferred until 16th December 2021 at Wimbledon Magistrates Court.

The result of the sentencing hearing was:

1. Fine for the outbuilding EN: £6,000, reduced by 10% so £5,400
2. Fine for the dormer EN: £12,000, reduced by 10% so £10,800
3. Surcharge: £181
4. Costs: £14,580
5. Total being £30,961. To be paid over a period of three years in monthly instalments.

The defendant was fined for the outbuilding and the dormer extensions due to non-compliance with two enforcement notices.

Existing enforcement appeals

2

Appeals determined

0

New Enforcement Appeals

0

3.4 Requested update from PAC

None

4. Consultation undertaken or proposed

None required for the purposes of this report

5 Timetable

N/A

6. Financial, resource and property implications

N/A

7. Legal and statutory implications

N/A

8. Human rights, equalities and community cohesion implications

N/A

9. Crime and disorder implications

N/A

10. Risk Management and Health and Safety implications.

N/A

11. Appendices – the following documents are to be published with this report and form part of the report Background Papers

N/A

12. Background Papers

N/A